

HISTORIC AND DESIGN REVIEW COMMISSION

September 02, 2015

Agenda Item No: 1

HDRC CASE NO: 2015-346
COMMON NAME: N San Saba from Houston to W Travis
CITY COUNCIL DIST.: 1
APPLICANT: Carmen Valera-Rivas/City of San Antonio
OWNER: Carmen Valera-Rivas/City of San Antonio
TYPE OF WORK: Capital improvements, site work and landscaping
REQUEST:

The applicant is requesting a Certificate of Appropriateness for approval to:

1. Construct curbs, ramps, sidewalks and driveway approaches along the first four hundred feet of the east side of N San Saba from W Houston to W Martin.
2. Mill and overlay from W Houston to W Martin and replace asphalt bus pads from W Commerce to W Houston and from W Houston to W Martin.
3. Remove and replace the existing inlet on the northeast corner of W Houston and N San Saba.
4. Landscape the area between the new curb and sidewalk to be installed.

APPLICABLE CITATIONS:

Sec. 35-646. - Construction in Public Rights-of-Way.

(a) General Provisions. All construction in the public right-of-way shall conform to all city codes. In considering an application, the historic and design review commission shall be guided by the following:

(1) Sidewalk Zones. Pedestrian movement should be pleasant, allowing for store browsing, comfortable transit waiting and easy accessibility for disabled people. Where possible, sidewalks should at least five (5) feet in width. Existing sidewalks should not be narrowed when replaced.

(2) Sidewalk Paving and Surfaces. Materials should complement stylistic differences of individual buildings, particularly when related to historic buildings.

A. Materials. Materials should be chosen for beauty, strength, longevity, easy maintenance and traction when dry or wet.

B. Color and Texture. To ensure the safety of pedestrians, all changes in surfaces should be defined by contrasting color, texture or materials.

FINDINGS:

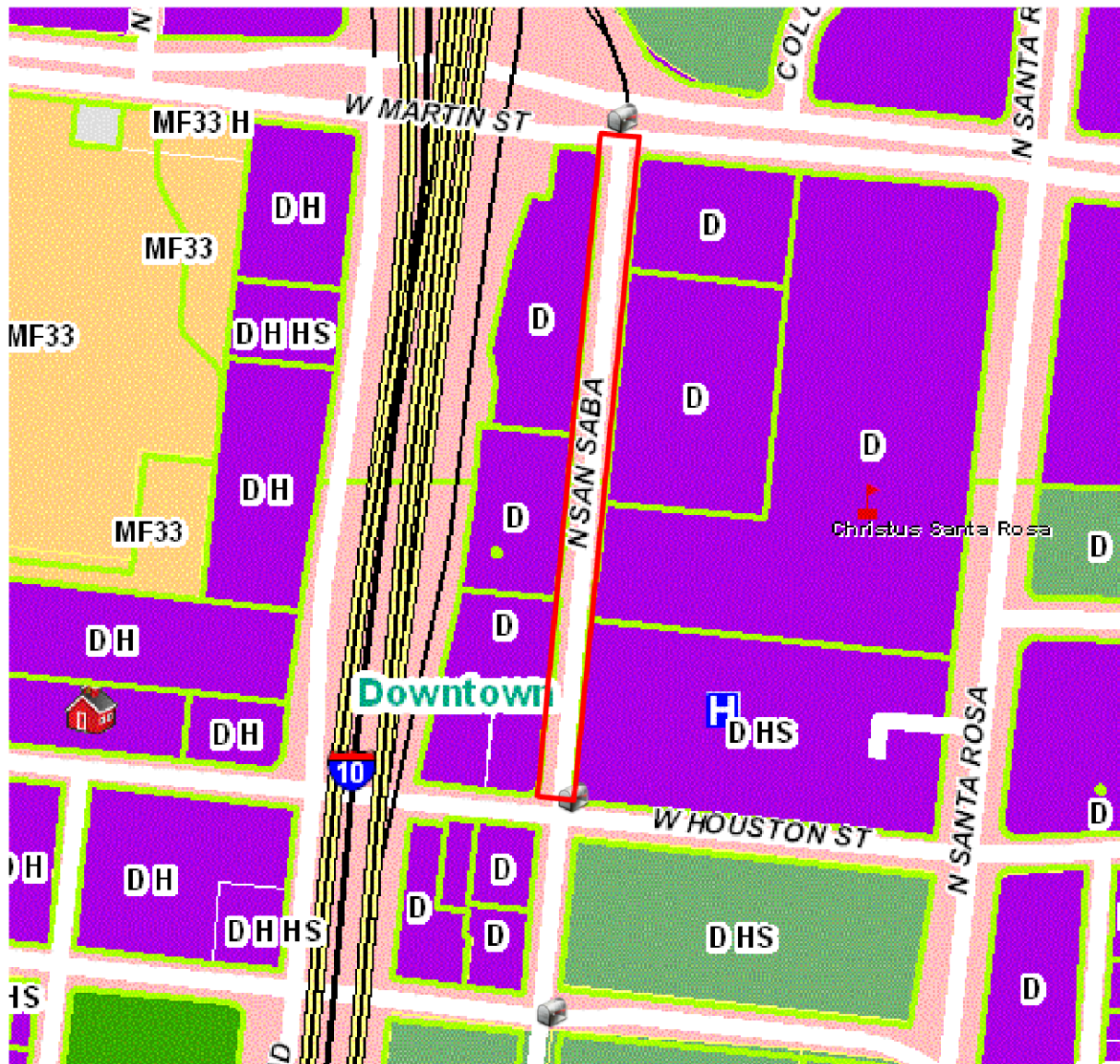
- a. The applicant has proposed various capital improvements to N San Saba from W Houston to W Martin which will improve pedestrian traffic and improve access to public transportation. This is consistent with the UDC Section 35-646.
- b. The applicant has proposed to install landscaping as a buffer between the the new curb and new sidewalk installations. The applicant has noted that this installation will only include shrubbery to be no taller than two and one half feet in height. This is consistent with the UDC.

RECOMMENDATION:

Staff recommends approval as submitted based on findings a and b.

CASE MANAGER:

Edward Hall



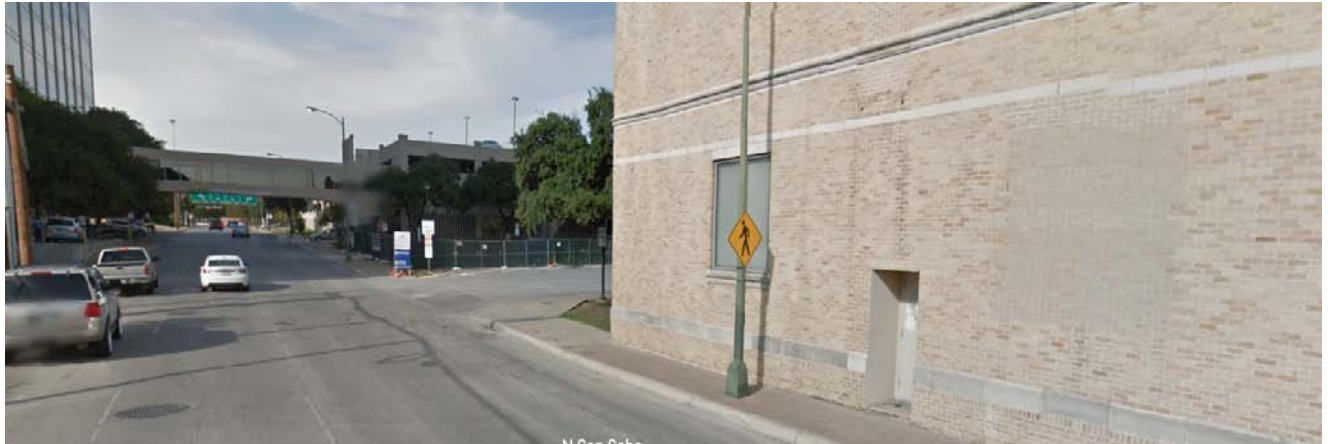
Flex Viewer

Powered by ArcGIS Server

Printed: Aug 25, 2015

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N. San Saba from W. Houston to W. Martin



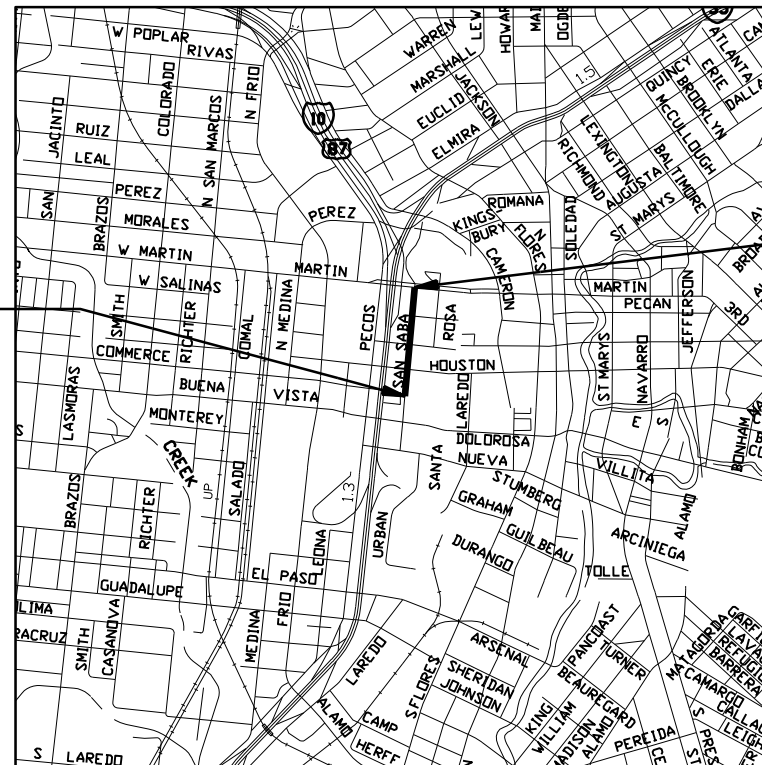


CITY OF SAN ANTONIO

TRANSPORTATION & CAPITAL IMPROVEMENTS

N. SAN SABA SIDEWALK IMPROVEMENTS (W. HOUSTON TO W. MARTIN)

BEGIN PROJECT
STA 07+15.00



LOCATION MAP - NOT TO SCALE

ROADWAY CLASSIFICATION: LOCAL TYPE A
DESIGN SPEED: 30 MPH
TYPE OF PROJECT: SIDEWALK IMPROVEMENTS

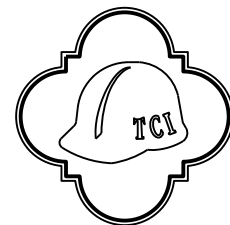
END PROJECT
STA 20+65.00

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Engineer: **FERNANDO CAMARILLO**
P.E. Serial No.: **86882**
Date: **7/24/2015**

PROJECT MANAGER

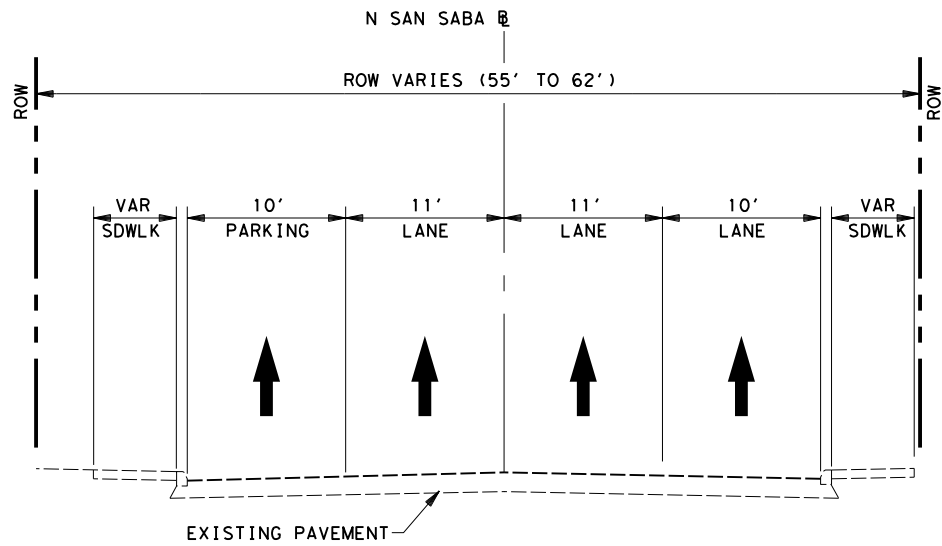


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(210) 349-3273 (PH)
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TBPE REGISTRATION NO. F-483

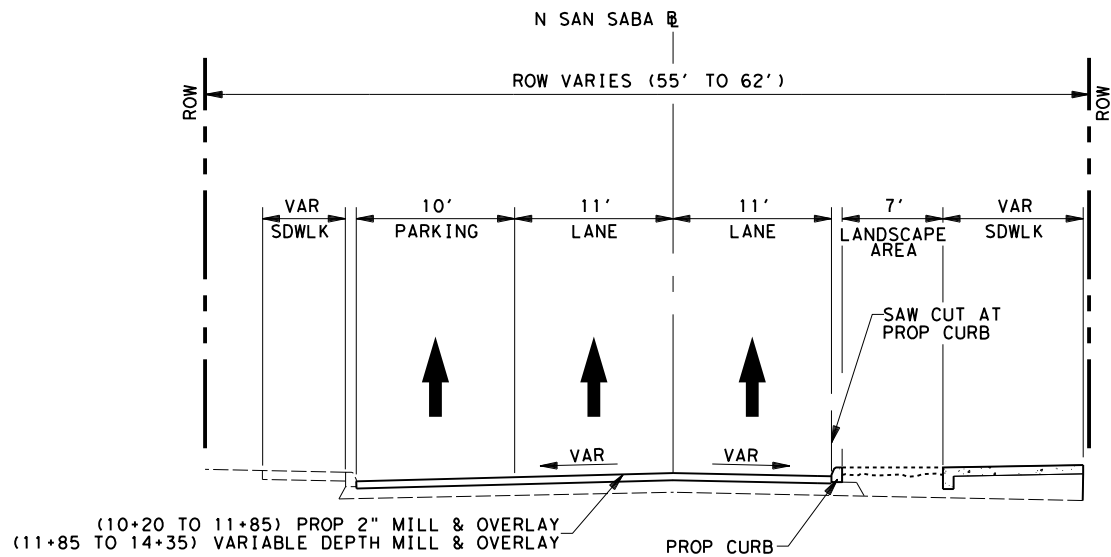


CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

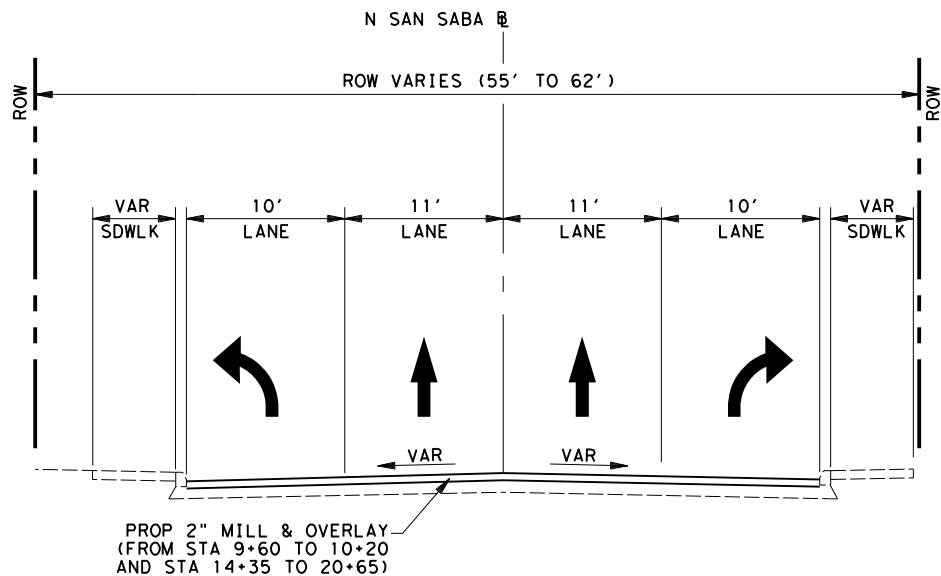
TDLR INSPECTION NOT REQUIRED



EXISTING N SAN SABA
FROM STA 07+15 TO 20+65

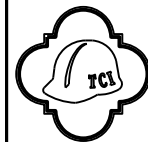


PROPOSED N SAN SABA
FROM STA 10+20 TO STA 14+35



PROPOSED N SAN SABA
FROM STA 07+15 TO 10+20
FROM STA 14+35 TO 20+65

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Engineer: FERNANDO CAMARILLO
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Date: 7/24/2015



CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

NO	DATE	DESCRIPTION REVISIONS	DWG	CHK

N SAN SABA SIDEWALK
IMPROVEMENTS
FROM W HOUSTON TO W MARTIN

EXISTING & PROPOSED
TYPICAL SECTIONS

N. T. S.

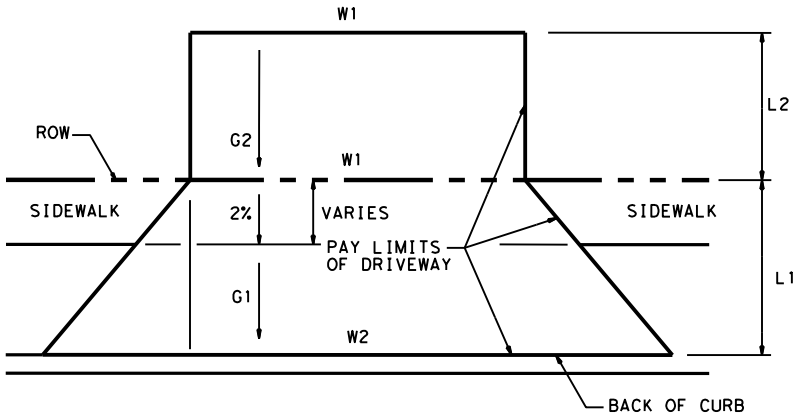
POZNECKI
CAMARILLO
INC

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DGN:				
CHK DGN:	CB			
DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	CB	TEXAS	BEXAR	2 OF 25

DW #	DRIVEWAY CENTERLINE STATION TO BASELINE	AREA (SY) COMMERCIAL	AREA (SY) RESIDENTIAL	LT	RT	PENETRATION (L2) (FT)	G1 (%)	G2 (%)	W1 @ ROW (FT)	W2 @Curb (FT)	CONCRETE RETAINING WALL (CY)	EXIST GATE AND TYPE TO BE ADJUSTED		EXISTING FENCE TYPE
1	12+14.09	50			X	0.00	1.59		18.80	41.80		TO REMAIN		TO REMAIN
2	13+58.43	113			X	0.00	1.85		46.50	69.20				
TOTAL		163.00	0.00								0.00	0.00		

NOTE: CONTRACTOR TO ADJUST GATE OF TYPE AS NOTED AND AS PER SPECIFICATION 523. EXISTING FENCE TO BE ADJUSTED AS NECESSARY TO MAINTAIN THE INTEGRITY OF THE FENCE. (N.S.P.I.)

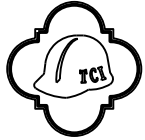


CONCRETE DRIVEWAY

NTS

SEE DRIVEWAY DETAILS AND PLAN AND PROFILE SHEETS FOR ADDITIONAL INFORMATION

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P.E. Serial No.: 86882
Date: 7/24/2015




CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

NO	DATE	DESCRIPTION REVISIONS	DWG	CHK

N SAN SABA SIDEWALK IMPROVEMENTS
FROM W HOUSTON TO W MARTIN
MISCELLANEOUS DETAILS AND SUMMARY

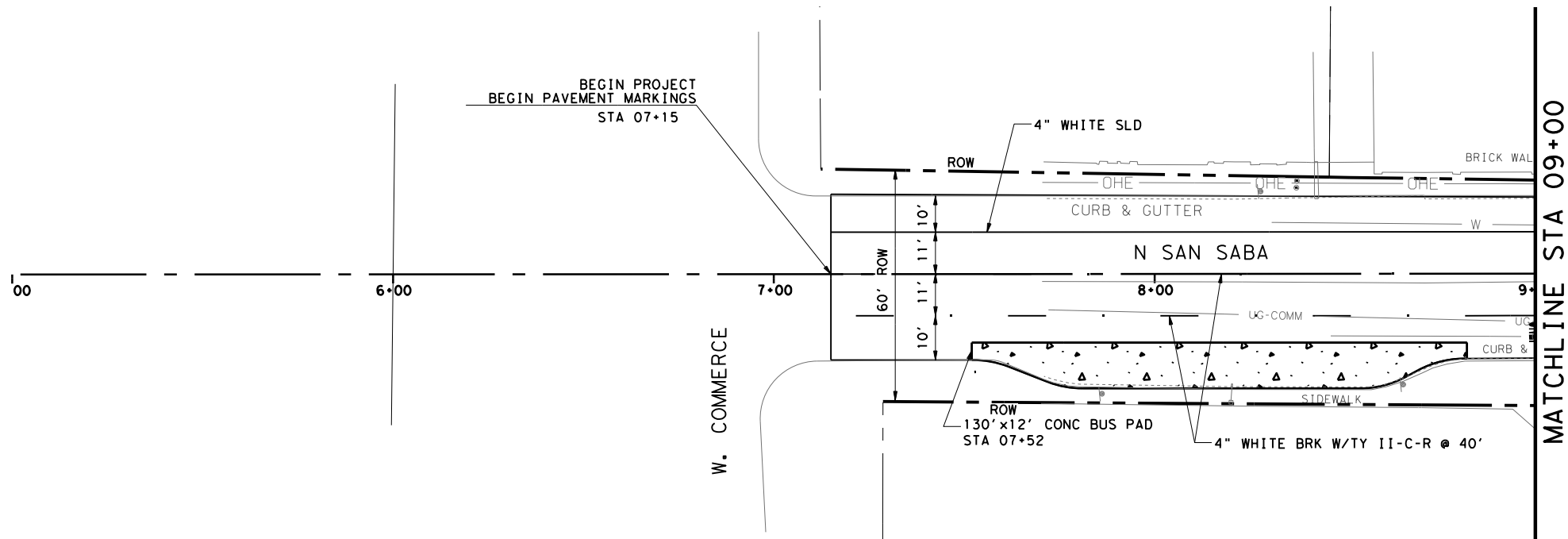
N. T. S.



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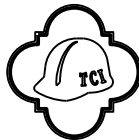
DGN:				
CHK DGN:	CB			
DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	CB	TEXAS	BEXAR	3 OF 25



- LEGEND**
- ROW --- RIGHT OF WAY/PROPERTY LINE
 - PROPSIGN RELOCATION
 - SURFACE RUNOFF FLOW DIRECTION
 - TRAFFIC FLOW DIRECTION
 - PROP CONC DRIVEWAY (COMMERCIAL)
 - PROP CONC SIDEWALK
 - TP TOP OF PAVEMENT ELEV

- NOTES:**
- THE INFORMATION SHOWN ON THIS DRAWING CONCERNING THE EXISTING OR PROPOSED LOCATION AND TYPE OF UNDERGROUND AND OVERHEAD UTILITIES IS FOR CONTRACTOR INFORMATION ONLY, FOR THE PURPOSE OF COORDINATION WITH UTILITY COMPANIES, AND IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OR HER OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES PRESENT AND MAKE NECESSARY MEASURES TO AVOID DAMAGE THERETO.
 - CALL THE TEXAS ONE CALL LOCATOR AT 1-800-344-8377, 48 HOURS BEFORE BEGINNING ANY EXCAVATION.
 - DUE TO FEDERAL REGULATION TITLE 49, PART 192.181, CPS MUST MAINTAIN ACCESS TO GAS VALVES AT ALL TIMES. THE CONTRACTOR MUST PROTECT AND WORK AROUND ANY GAS VALVES THAT ARE IN THE PROJECT AREA.
 - THE CONTRACTOR WILL BE RESPONSIBLE FOR PROTECTING CPS OVERHEAD AND UNDERGROUND ELECTRICAL FACILITIES IF ADJACENT TO WORK AREA. PART 192.181.
 - ALL EXISTING UTILITIES SHOWN ON THIS SHEET ARE POSITIONED WITH A CONFIDENCE OF QUALITY LEVEL "C" UNLESS OTHERWISE NOTED. SERVICE LINES FOR GAS, WATER AND SANITARY SEWER ARE SHOWN TO QUALITY LEVEL "C".
 - CONTROL POINTS FOR MANHOLES AND JUNCTION BOXES SHALL BE TO THE CENTER OF THE STRUCTURE BASE.
 - CONTROL POINTS TO THE INLETS SHALL BE TO THE MIDPOINT OF THE FACE OF CURB FOR CURB INLET ONLY (EXCLUDES EXTENSIONS). TOP OF CURB INLET ELEVATIONS SHALL MATCH THE PROPOSED TOP OF CURB ELEVATIONS.
 - ALL DRAINAGE PIPE SHALL BE RCP CLASS III UNLESS NOTED OTHERWISE
 - ALL BENDS, TEES AND WYES, JOINT SEALS, CONNECTIONS, AND MODIFICATIONS NECESSARY FOR PROPER INSTALLATION OF STORM DRAINAGE SYSTEMS SHALL BE SUBSIDIARY TO RCP AND BOX CULVERT BID ITEMS.

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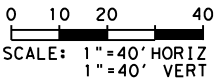


CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

NO	DATE	DESCRIPTION	DWG	CHK
REVISIONS				

**N SAN SABA SIDEWALK
IMPROVEMENTS**
FROM W HOUSTON TO W MARTIN

PLAN AND PROFILE
BEGIN PROJECT TO STA. 09+00

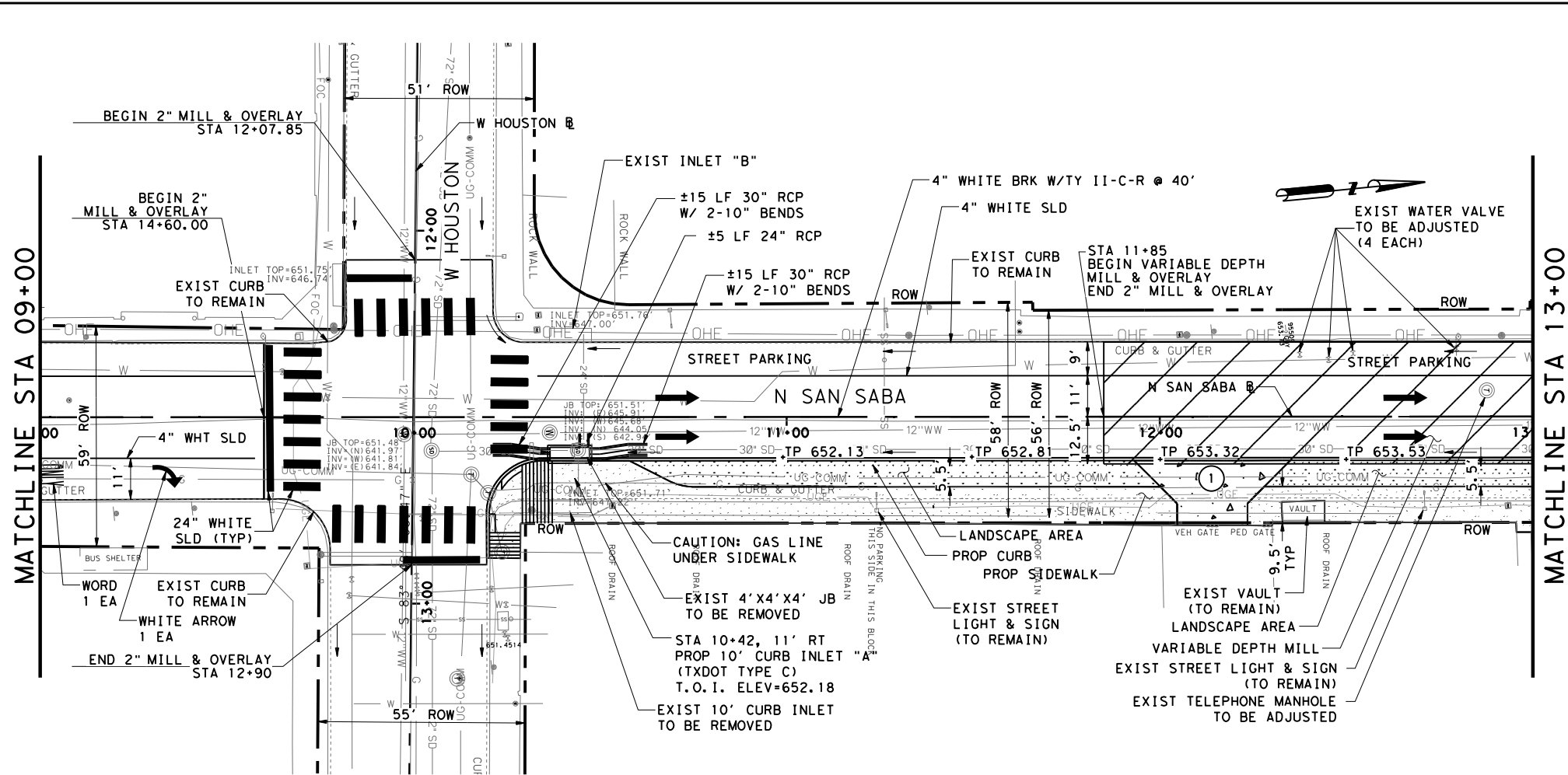


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DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	CB	TEXAS	BEXAR	4 OF 25



LEGEND

ROW --- RIGHT OF WAY/PROPERTY LINE

PRO SIGN RELOCATION

SURFACE RUNOFF FLOW DIRECTION

TRAFFIC FLOW DIRECTION

PROP CONC DRIVEWAY (COMMERCIAL)

PROP CONC SIDEWALK

TP TOP OF PAVEMENT ELEV

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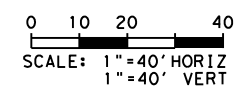
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NO	DATE	DESCRIPTION	DWG	CHK
REVISIONS				

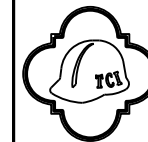
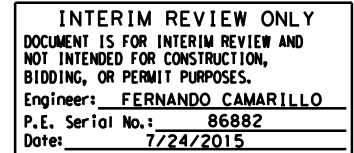
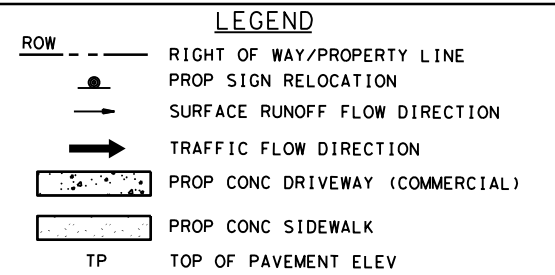
N SAN SABA SIDEWALK IMPROVEMENTS
FROM W HOUSTON TO W MARTIN

PLAN AND PROFILE
BEGIN PROJECT TO STA. 13+00



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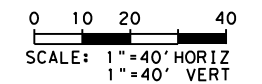
DGN:				
CHK DGN:	CB			
DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	CB	TEXAS	BEXAR	5 OF 25

CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

NO	DATE	DESCRIPTION	DWG	CHK	
		REVISIONS			

**N SAN SABA SIDEWALK
IMPROVEMENTS
FROM W HOUSTON TO W MARTIN**

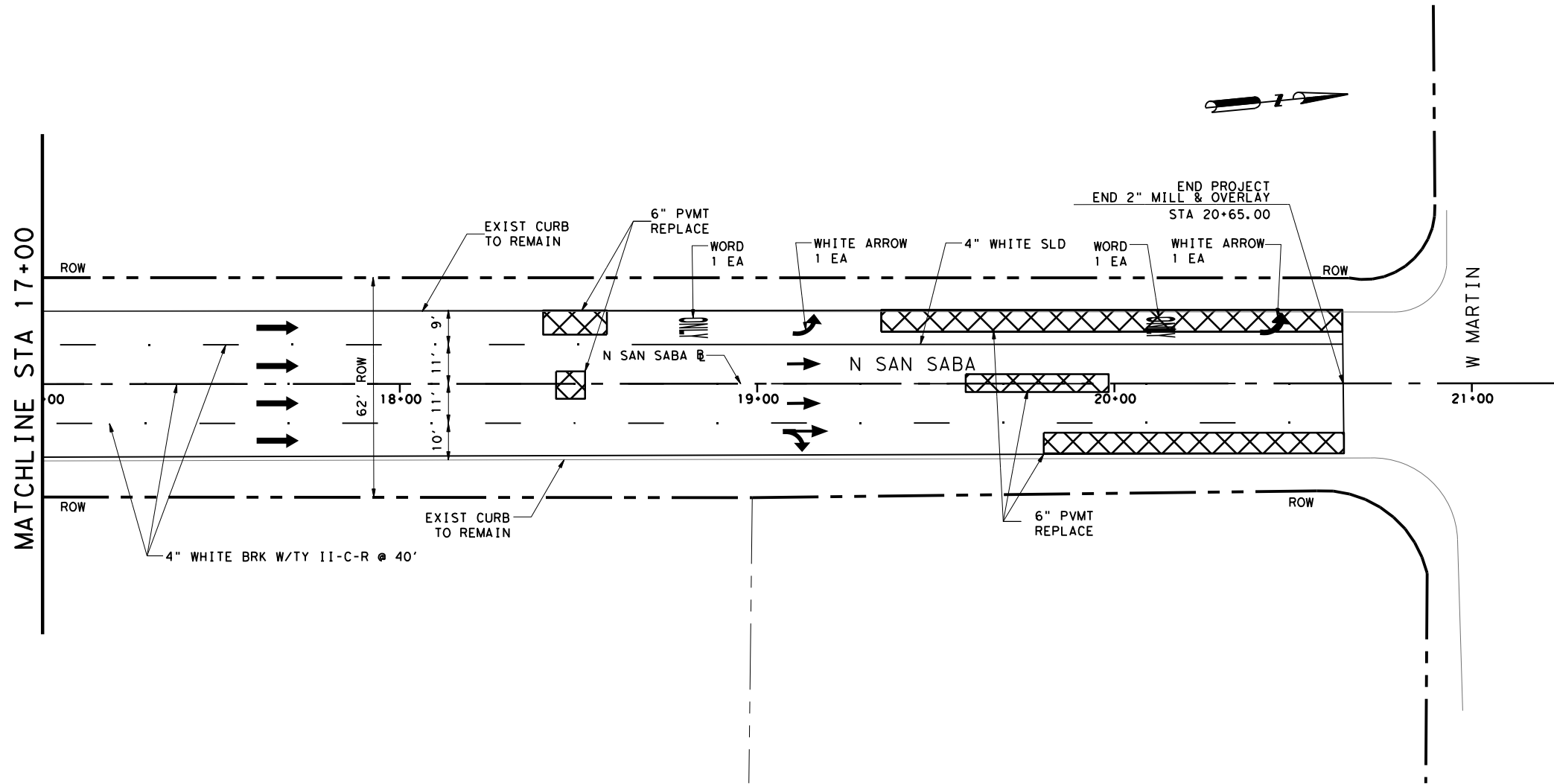
PLAN AND PROFILE
STA. 13+00 TO STA. 17+00



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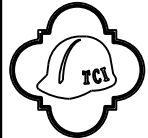
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CHK DGN:	CB			
DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	CB	TEXAS	BEXAR	6 OF 25

Design File Name: \\pc1server2\datagate\Jobs\13\020\Techprod\San_Saba\CapImprove\ROADWAY\SABA-PP-SHEETS.dgn
Plotted on: 7/24/2015 1:36:14 PM



- LEGEND**
- ROW --- RIGHT OF WAY/PROPERTY LINE
 - PROPOSED SIGN RELOCATION
 - SURFACE RUNOFF FLOW DIRECTION
 - TRAFFIC FLOW DIRECTION
 - PROPOSED CONC DRIVEWAY (COMMERCIAL)
 - PROPOSED CONC SIDEWALK
 - TP TOP OF PAVEMENT ELEV

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CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS


NO	DATE	DESCRIPTION REVISIONS	DWG	CHK

N SAN SABA SIDEWALK IMPROVEMENTS
FROM W HOUSTON TO W MARTIN

PLAN AND PROFILE
STA. 17+00 TO END PROJECT

0 10 20 40

SCALE: 1"=40' HORIZ
1"=40' VERT


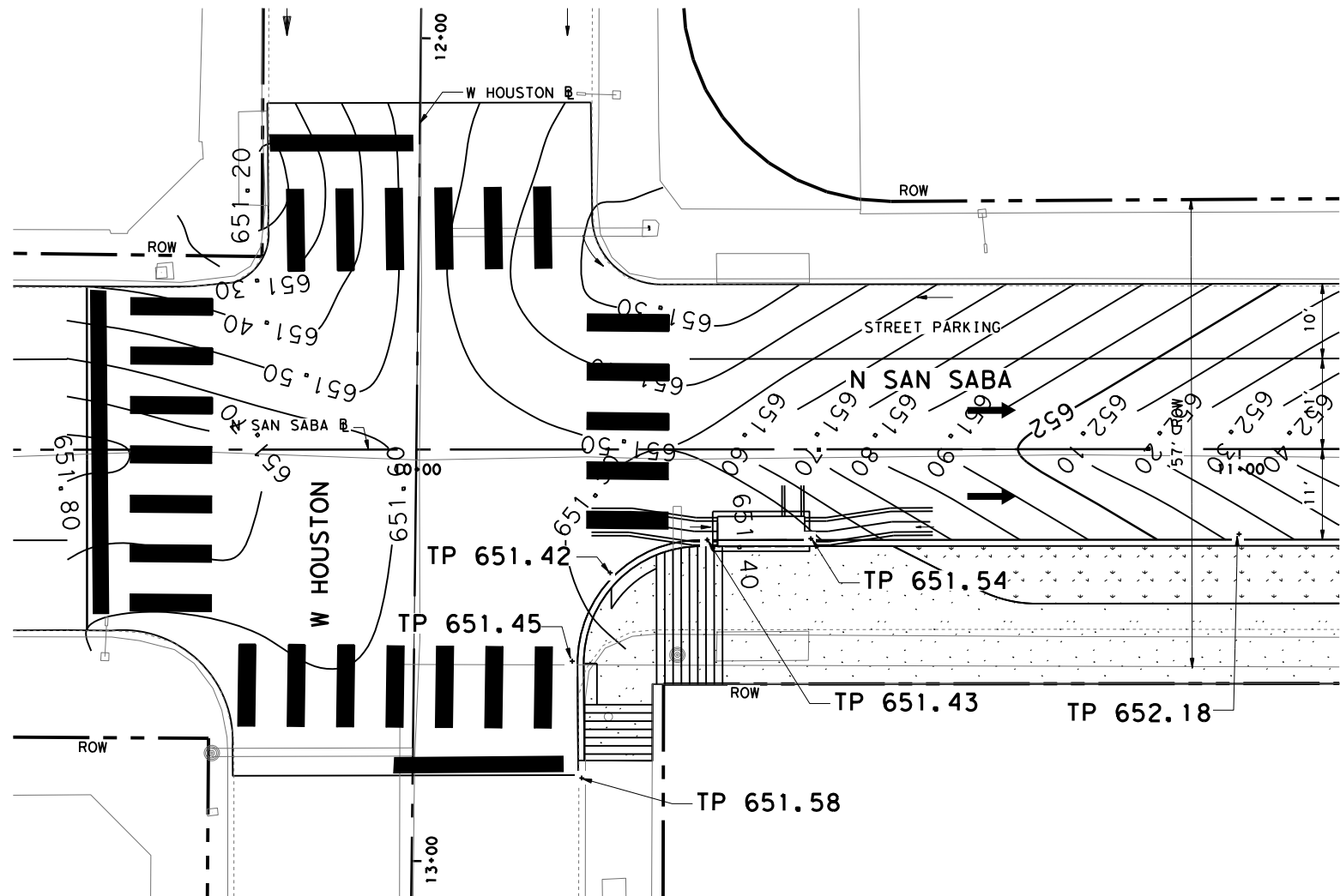


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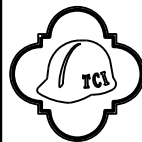
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DGN				
CHK				
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DWG		STATE	COUNTY	SHEET NO.
CHK	CB	TEXAS	BEXAR	7 OF 25
DWG				



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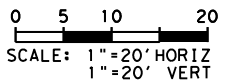


CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

NO	DATE	DESCRIPTION	DWG	CHK	
		REVISIONS			

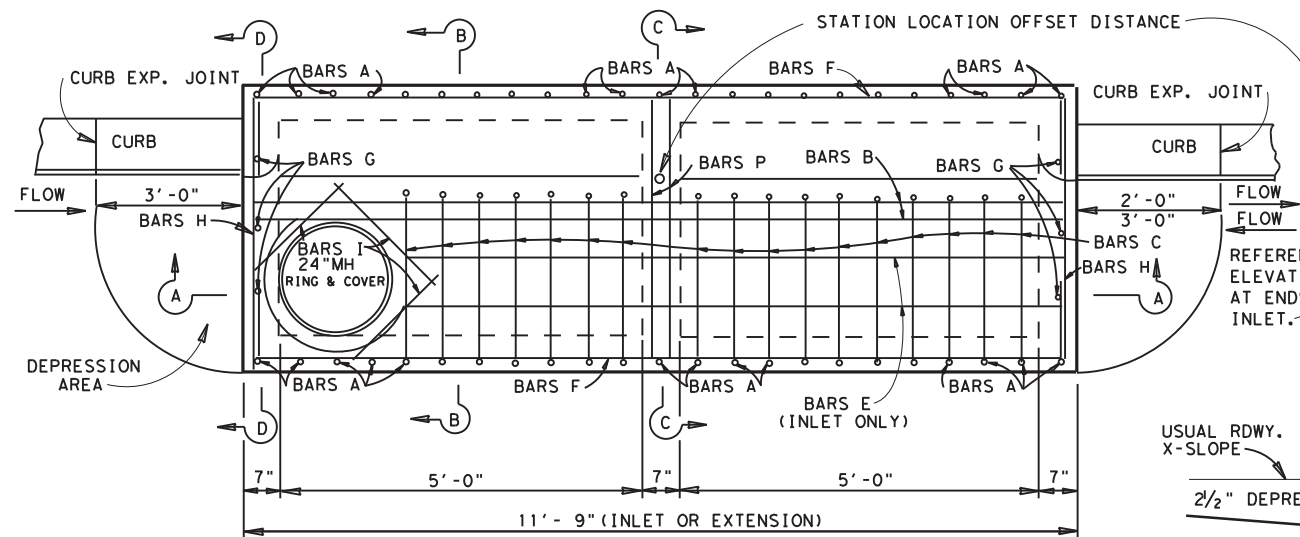
N SAN SABA SIDEWALK IMPROVEMENTS FROM W HOUSTON TO W MARTIN

INTERSECTION LAYOUT
SAN SABA & W HOUSTON



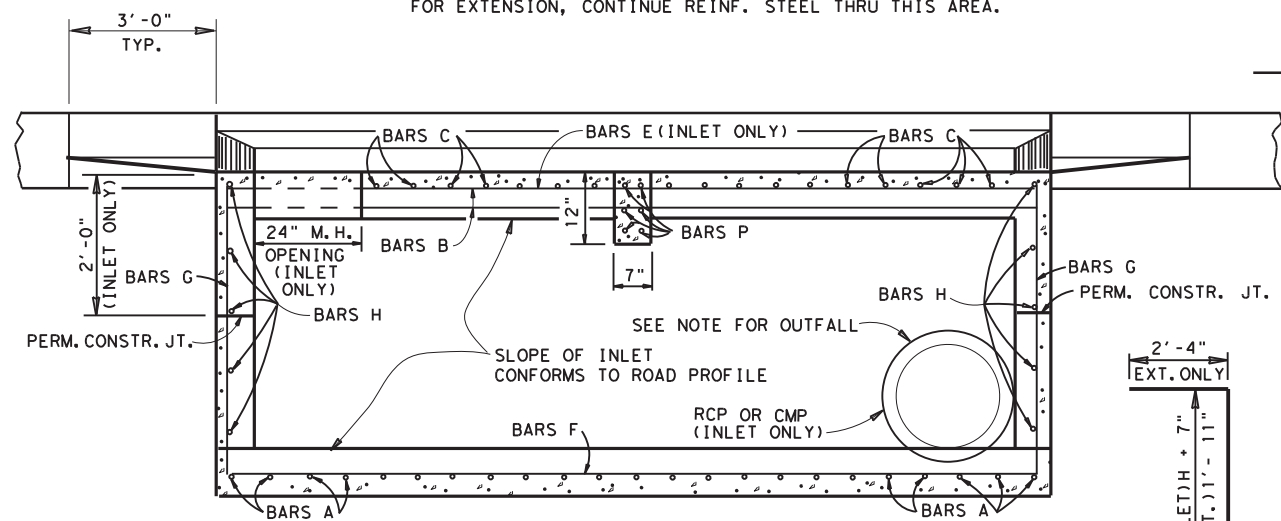
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CHK DWG:	CB	TEXAS	BEXAR	8 OF 25

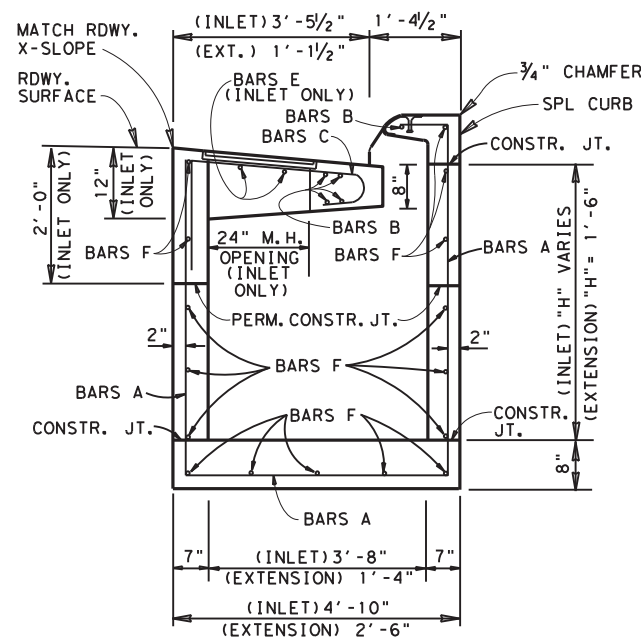


PLAN
(INLET OR EXTENSION)

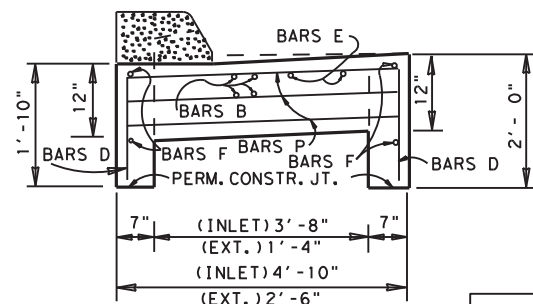
NOTE: RING & COVER SHOWN ABOVE IS TO BE PLACED IN INLET ONLY. FOR EXTENSION, CONTINUE REINF. STEEL THRU THIS AREA.



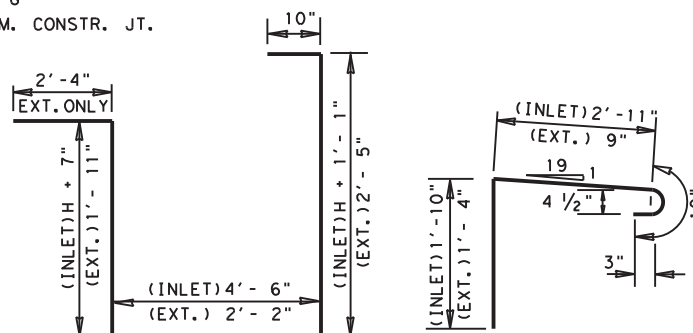
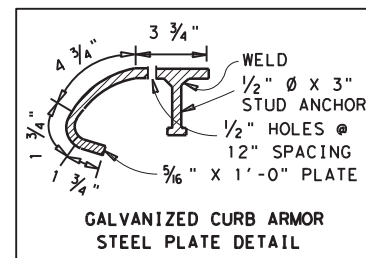
SECTION A-A



SECTION B-B

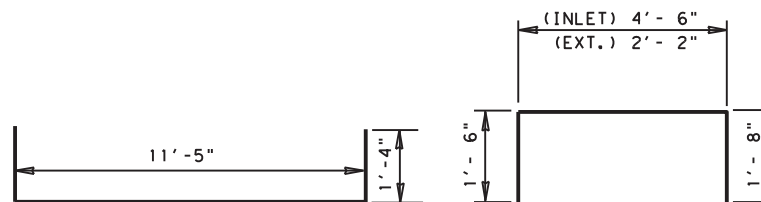


SECTION C-C
BEAM DETAIL



BAR A

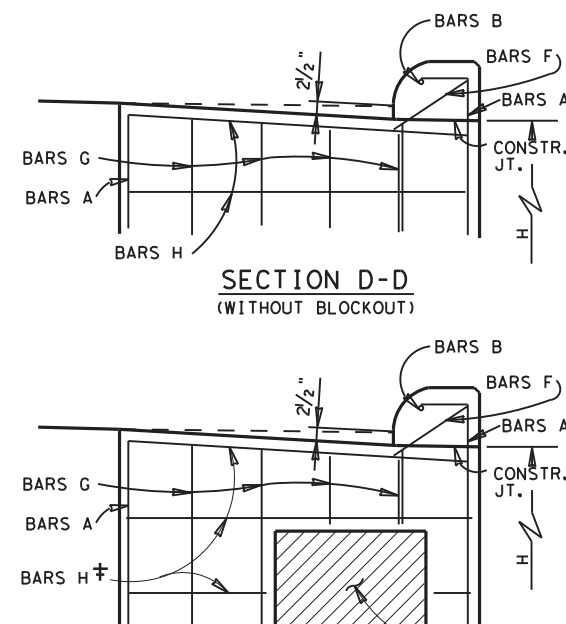
BAR C



BAR F

BAR D

BAR DETAILS

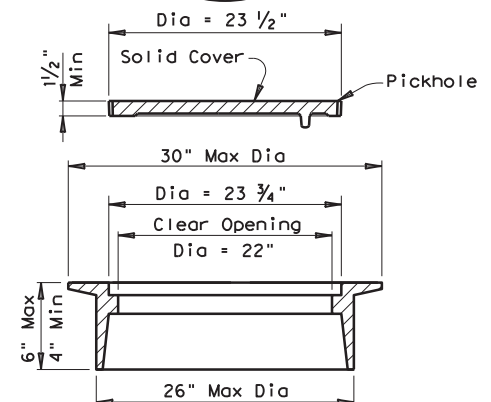


† CUT OR BEND STEEL FOR BLOCKOUT AREA.

SECTION D-D
(WITH BLOCKOUT)

REINFORCING STEEL ESTIMATED QUANTITIES - FOR "H" = 4'-10"					
BAR	NO.	SIZE	SPAC	LENGTH	WEIGHT
A	29	#4	5"	16'-8"	323
B	5	#5	3"	11'-5"	60
C	17	#5	6"	5'-7"	99
D	2	#5	-	7'-8"	16
E	2	#4	8"	9'-5"	13
F	17	#4	11"	14'-1"	160
G	6	#4	11"	4'-11"	20
H	10	#4	11"	4'-4"	29
I	3	#4	-	2'-4"	5
P	6	#5	-	4'-4"	27
TOTAL - REINF. STEEL - LBS.					752 *
TOTAL - CLASS "C" CONC. - CY.					5.60 *

* FOR CONTRACTOR'S INFORMATION ONLY. NO DEDUCTION MADE FOR PIPES. QUANTITIES SHOWN ARE FOR "H" = 4'-10". "H" MAY VARY



RING AND COVER DETAILS

Approximate Weight = 255 lb

GENERAL NOTES :

CURB MUST BE CAST-IN-PLACE FROM BEGIN GUTTER TRANSITION TO END GUTTER TRANSITION INCLUDING OVER INLET.

OUTFALL PIPES WHICH LAY TRANSVERSE TO ROADWAY, AT INLETS ON GRADE, WILL BE PLACED AT LOW END OF INLET. OTHER PIPES MAY BE PLACED ON ANY SIDE OF INLET.

ALL EXPOSED CORNERS SHALL HAVE A 3/4" CHAMFER.

FOR REINFORCING STEEL AND CONCRETE QUANTITIES FOR THE EXTENSION, SEE SHEET 2 OF 2.

IF THE ROADWAY CURB HEIGHT DOES NOT MATCH THE TOP OF THE INLET, TRANSITION THE ROADWAY CURB AT 40:1

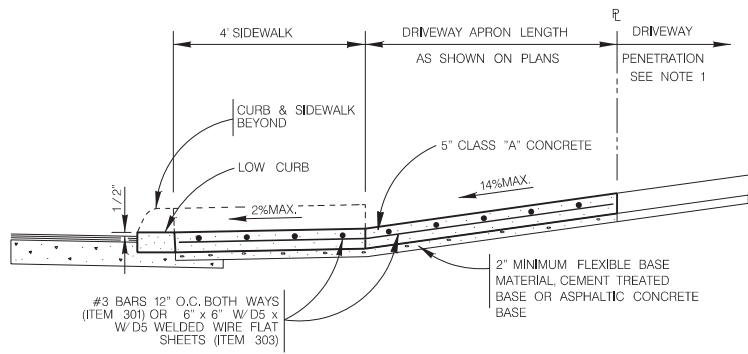
PAYMENT FOR CONC., REINF. STEEL, M H RING & COVER, CURB ARMOR AND STEPS SHALL BE INCLUDED IN UNIT COST OF ITEM 465 "MANHOLE & INLETS".

SAN ANTONIO DISTRICT STANDARD CURB INLET TYPE C WITH INLET EXTENSION TYPE C-E

© 1999 Texas Department of Transportation

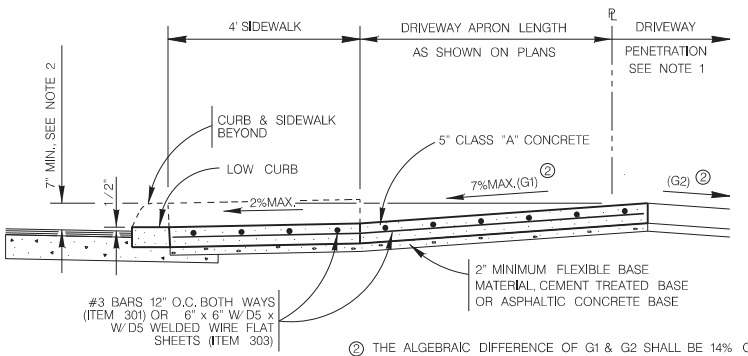
SHEET 1 OF 2

FED. RD. DIV. NO.	PROJECT NO.	SHEET NO.
6		9 OF 25
STATE	DISTRICT	COUNTY
TEXAS	SAT	
CONT.	SECT.	JOB
		HIGHWAY NO.



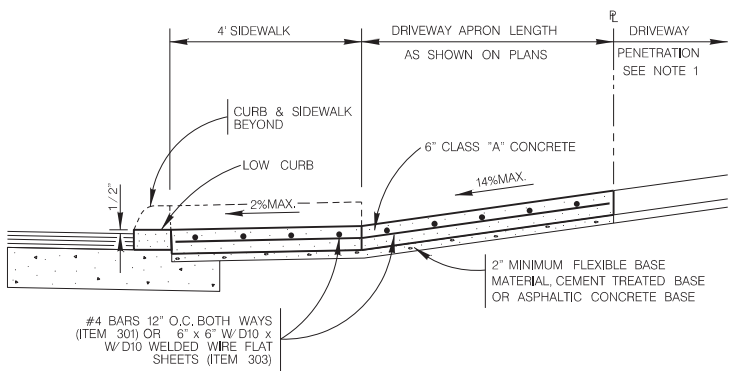
TYPICAL RESIDENTIAL DRIVEWAY SECTION

WITH SIDEWALK ABUTTING CURB
ITEM 503.1



TYPICAL RESIDENTIAL DRIVEWAY SECTION

WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS ABUTTING CURB
ITEM 503.1



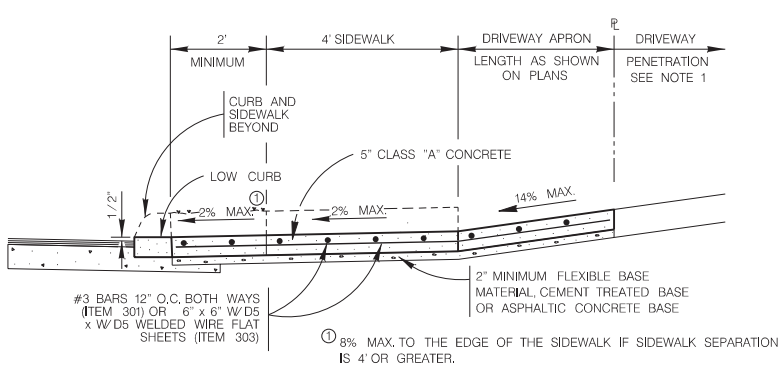
TYPICAL COMMERCIAL DRIVEWAY SECTION

WITH SIDEWALK ABUTTING CURB
ITEM 503.2

CONCRETE DRIVEWAY NOTES

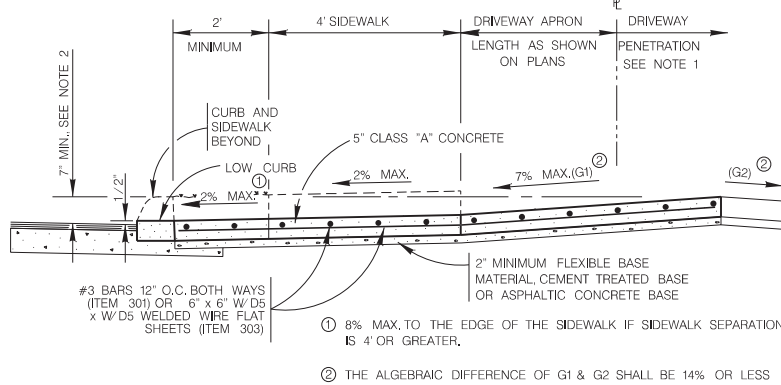
- DRIVEWAY PENETRATION REFERS TO A PORTION OF THE DRIVEWAY THAT MAY BE NECESSARY TO RECONSTRUCT WITHIN PRIVATE PROPERTY TO COMPLY WITH A MAXIMUM DRIVEWAY SLOPE. THIS PORTION OF THE DRIVEWAY SHALL BE PAID FOR UNDER THE FOLLOWING ITEMS AS MAY APPLY:
A.) CONCRETE DRIVEWAY PAID FOR UNDER ITEM NO.503.1 OR 503.2.
B.) ASPHALTIC CONCRETE DRIVEWAY PAID FOR UNDER ITEM NO.503.4 AND SHALL INCLUDE A MINIMUM OF 1\"/>

TYPE	MINIMUM	MAXIMUM
RESIDENTIAL	10'	20'
COMMERCIAL – ONE WAY	12'	20'
COMMERCIAL – TWO WAY	24'	30'



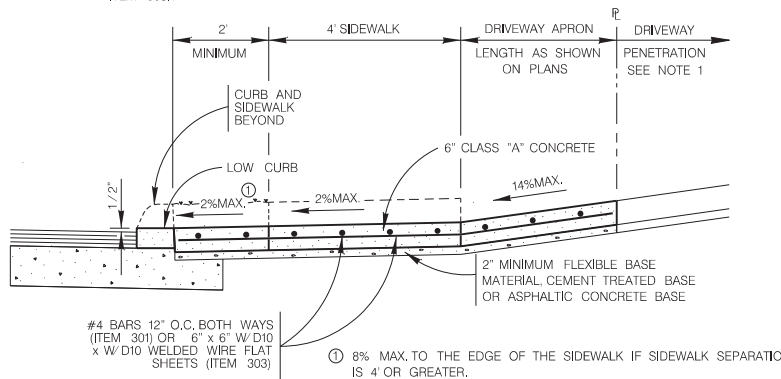
TYPICAL RESIDENTIAL DRIVEWAY SECTION

WITH SIDEWALK SEPARATED FROM CURB
ITEM 503.1



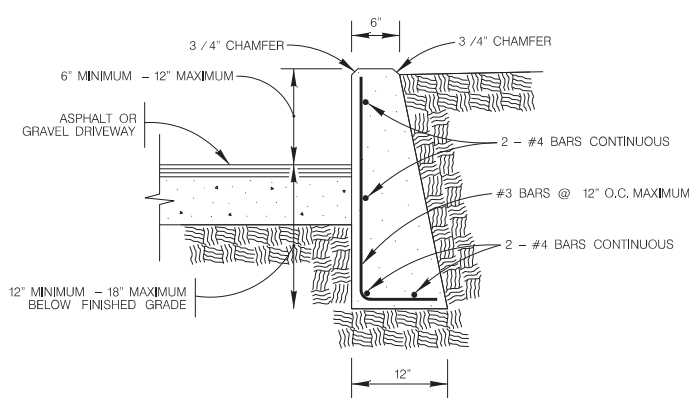
TYPICAL RESIDENTIAL DRIVEWAY SECTION

WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS SEPARATED FROM CURB
ITEM 503.1



TYPICAL COMMERCIAL DRIVEWAY SECTION

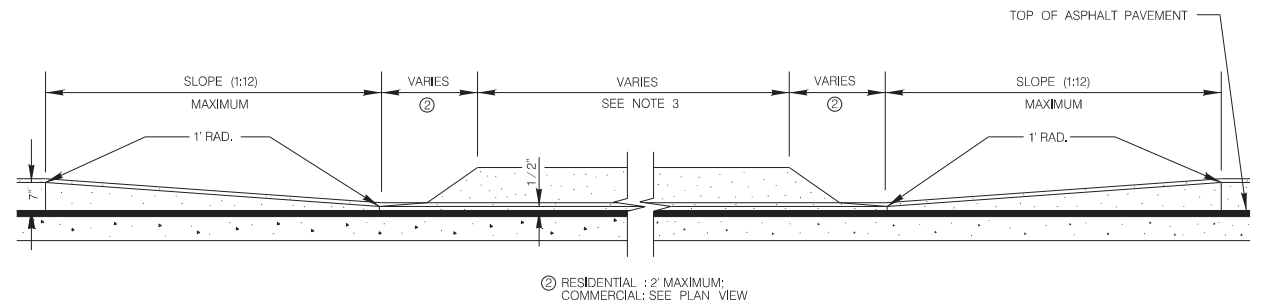
WITH SIDEWALK SEPARATED FROM CURB
ITEM 503.2



- NOTE:
- COST OF REINFORCEMENT TO BE INCLUDED IN UNIT COST OF ITEM 307.1.
 - CONCRETE RETAINING WALL COMBINATION TYPE SHALL BE USED FOR CONCRETE DRIVEWAYS.

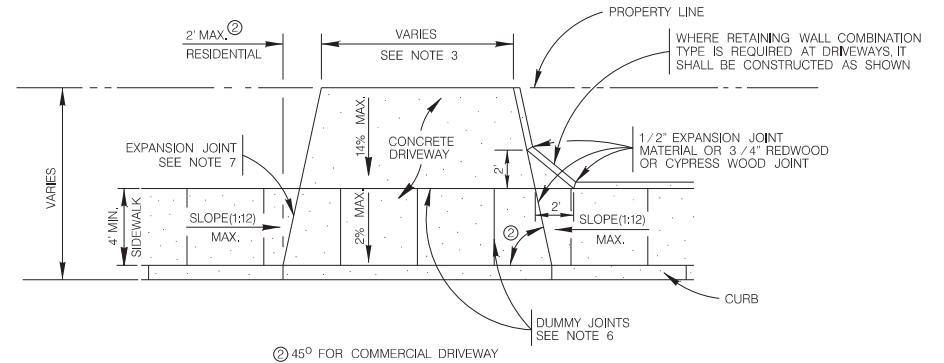
DRIVEWAY – CONCRETE RETAINING WALL

ON COMPACTED SUBGRADE
ITEM 307.1



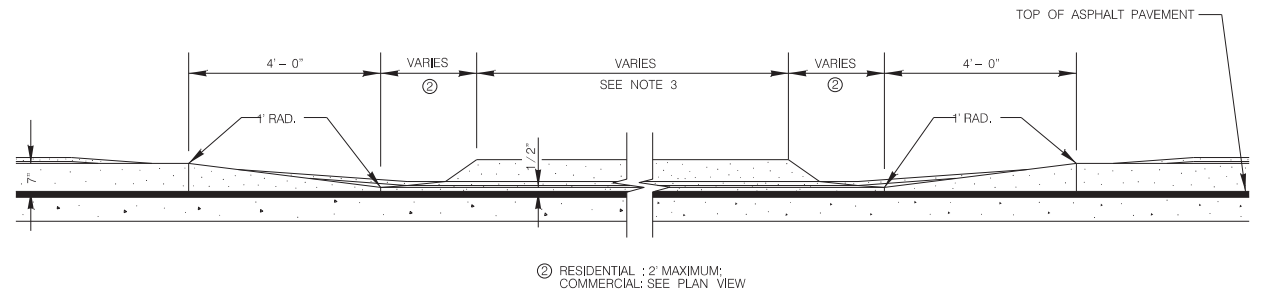
CURB PROFILE AT DRIVEWAY

WITH SIDEWALK ABUTTING CURB



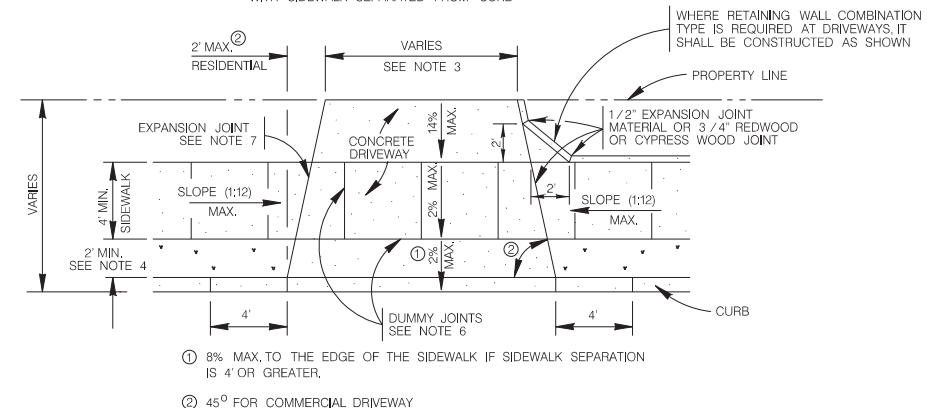
TYPICAL DRIVEWAY PLAN VIEW

WITH SIDEWALK ABUTTING CURB



CURB PROFILE AT DRIVEWAY

WITH SIDEWALK SEPARATED FROM CURB



TYPICAL DRIVEWAY PLAN VIEW

WITH SIDEWALK SEPARATED FROM CURB

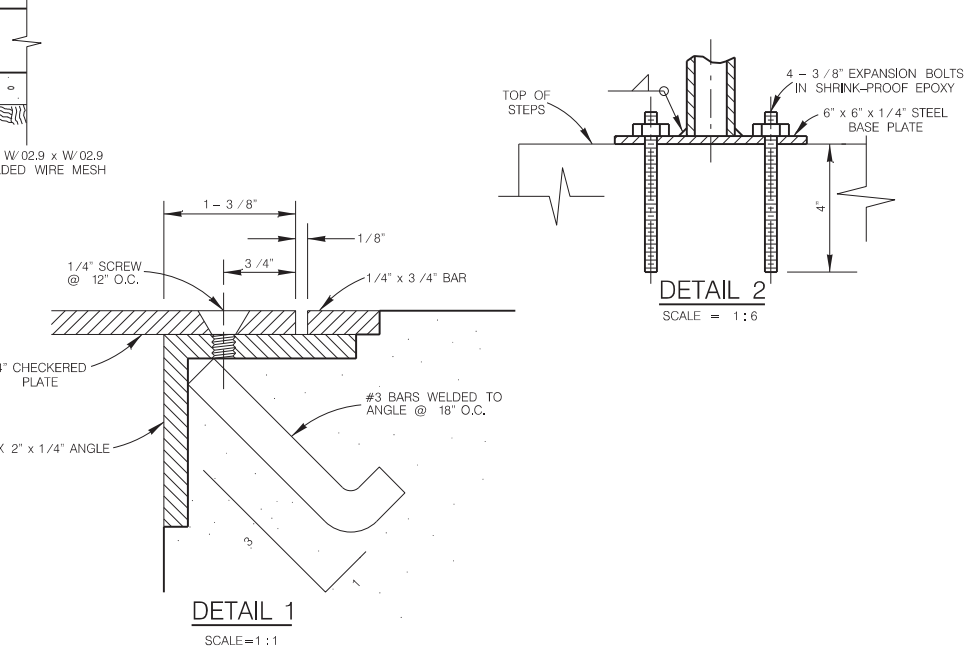
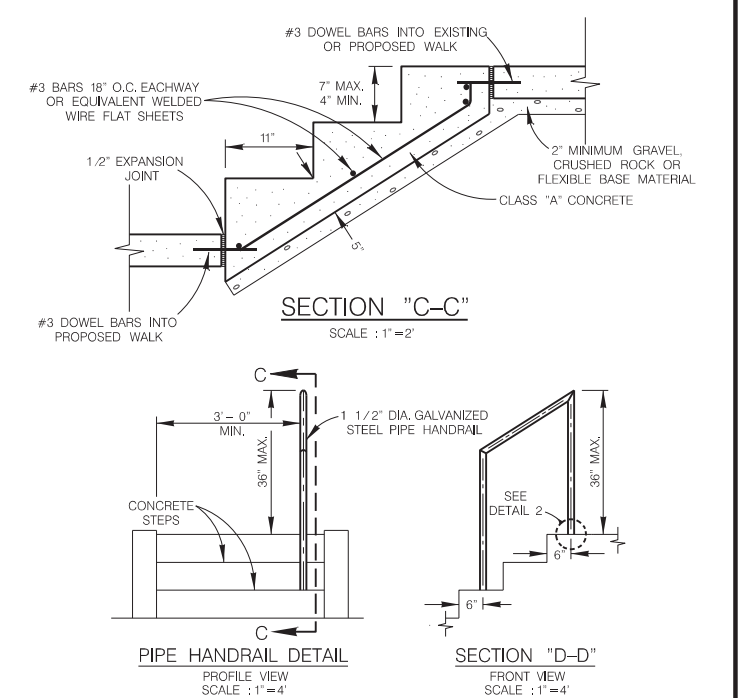
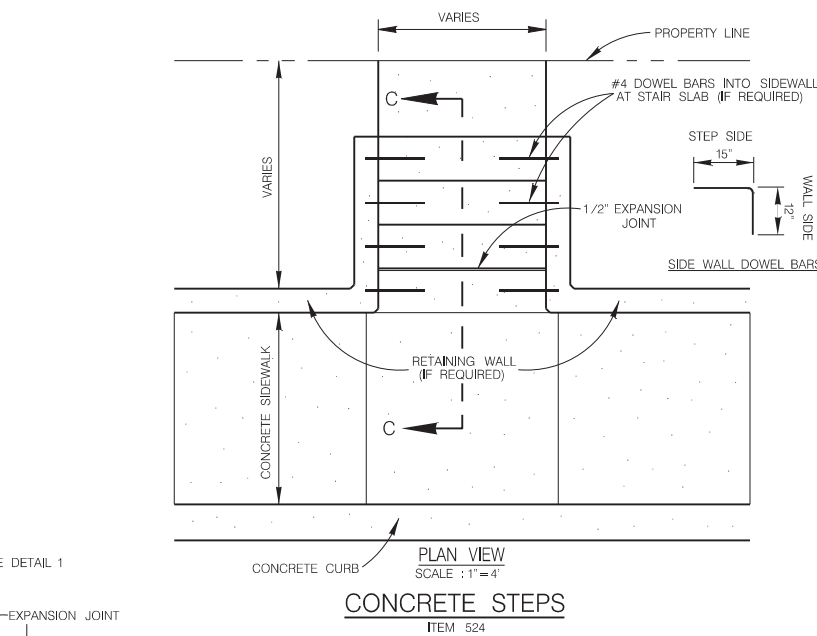
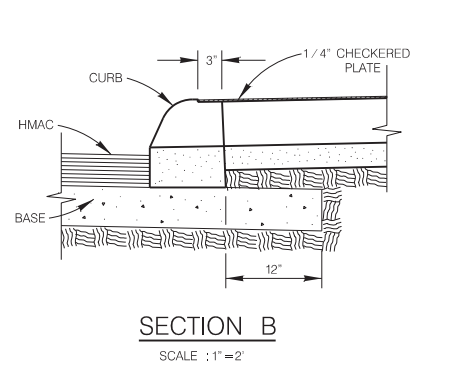
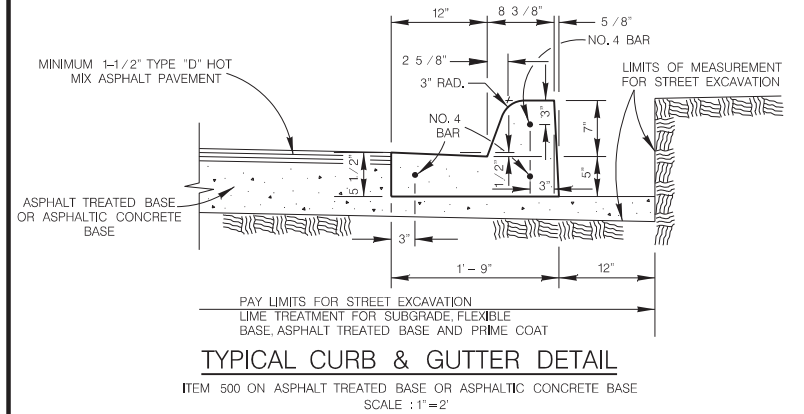
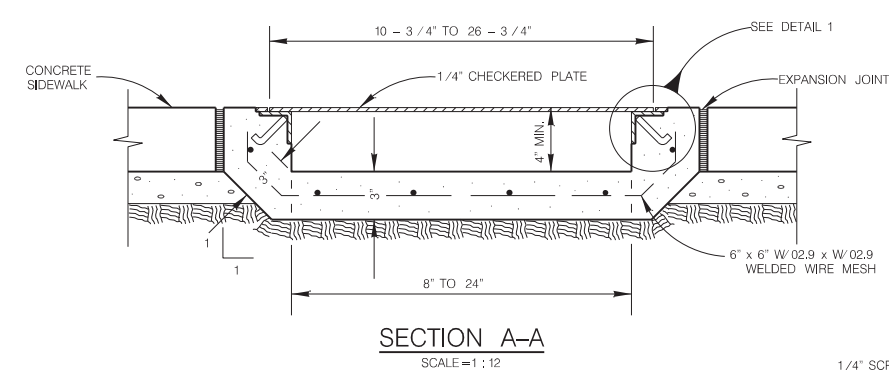
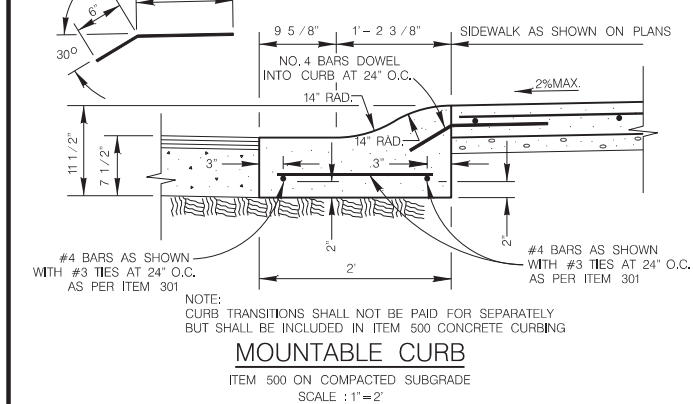
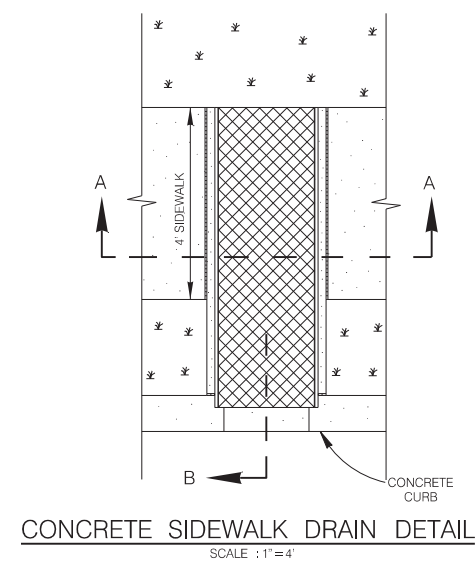
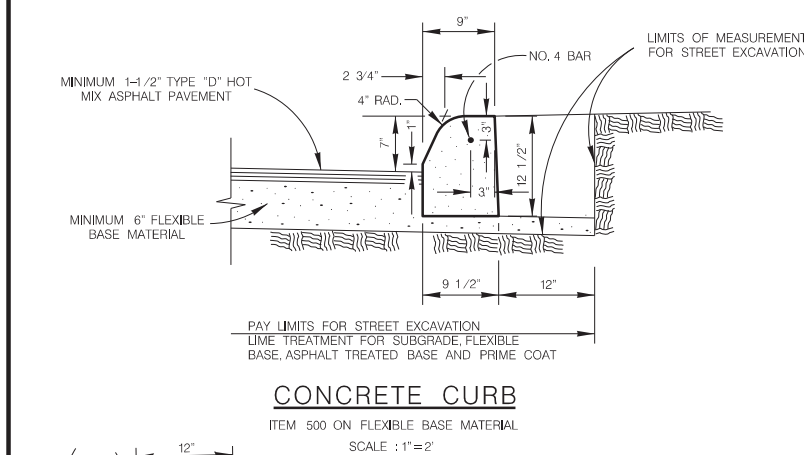
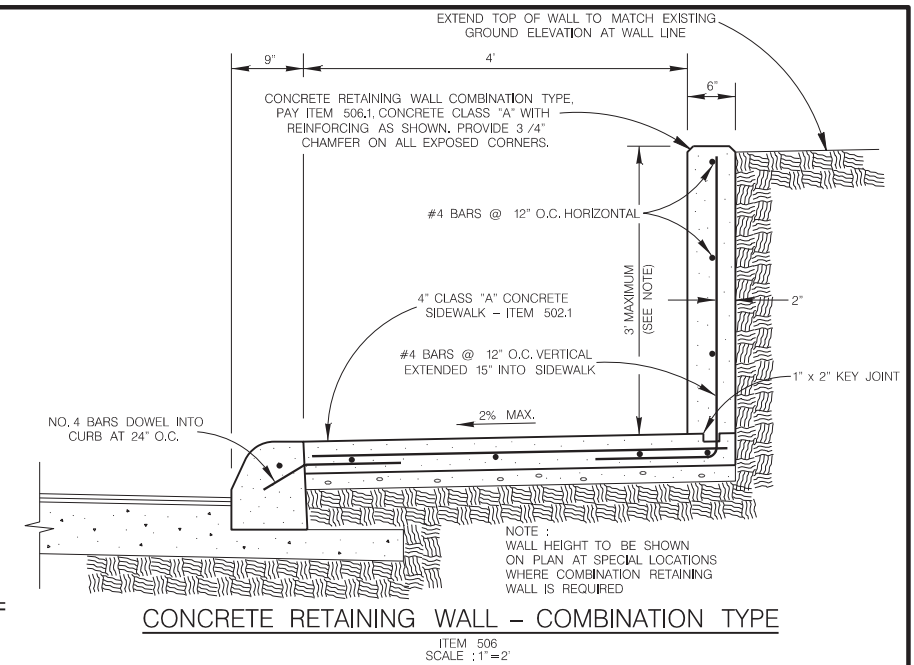
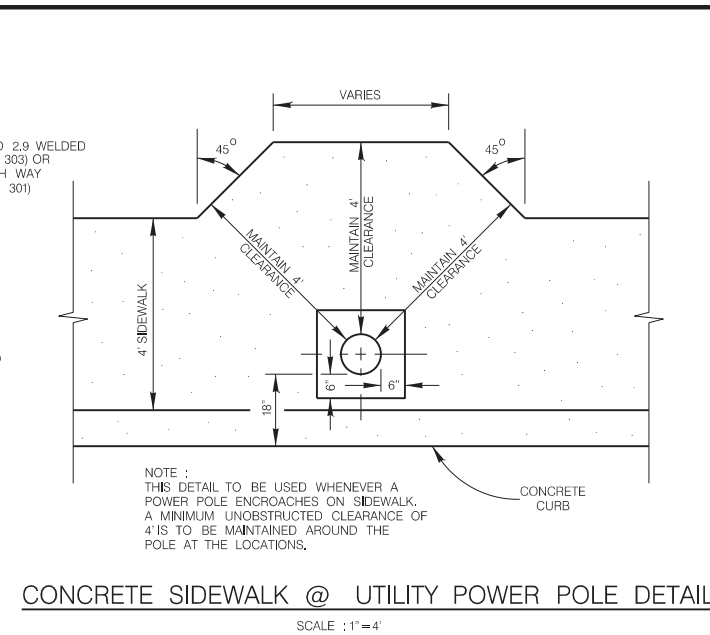
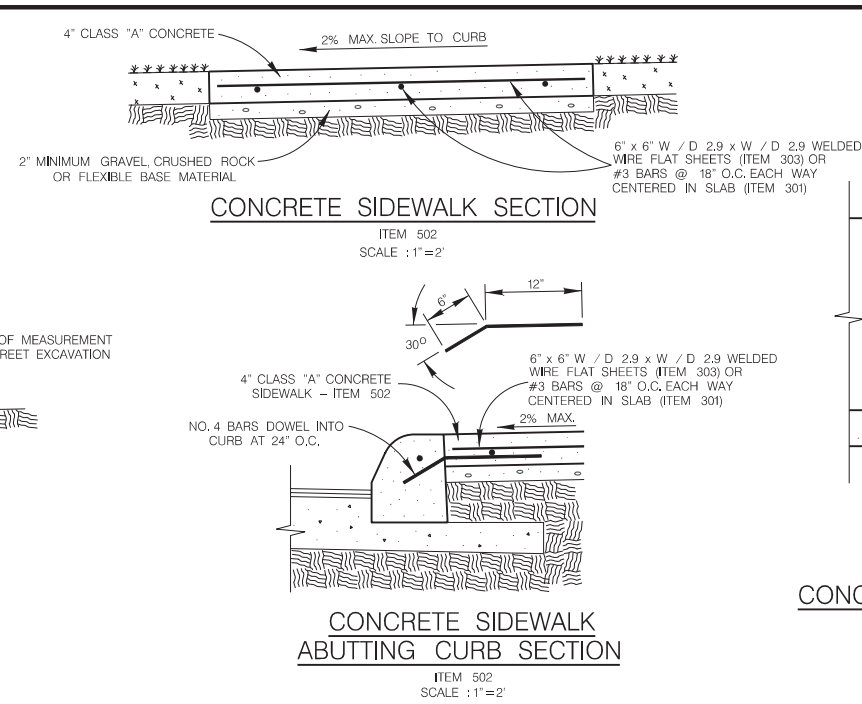
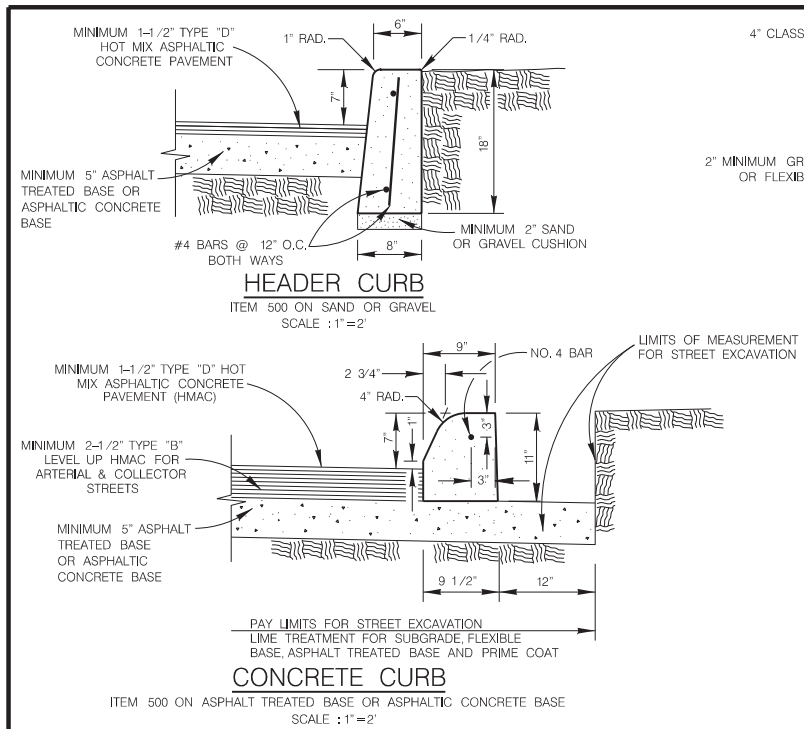
MAY 2009

CITY OF SAN ANTONIO
CAPITAL IMPROVEMENTS MANAGEMENT SERVICES DEPARTMENT

CONCRETE DRIVEWAY STANDARDS

% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: V. VASQUEZ	DSGN. BY:	CHKD. BY: R.S. HOSSEINI, P.E.

SHEET NO.: 10 OF 25



- NOTES :
1. PLACE STEPS TO CONFORM WITH PROPOSED PARKWAY GRADING.
 2. 1-1/2" DIA. GALVANIZED STEEL PIPE HANDRAIL REQUIRED ON ONE SIDE OF THE STEPS WHEN 3 OR MORE RISERS ARE USED, OR AS DESIGNED BY ENGINEER, TO BE PAID UNDER ITEM 522

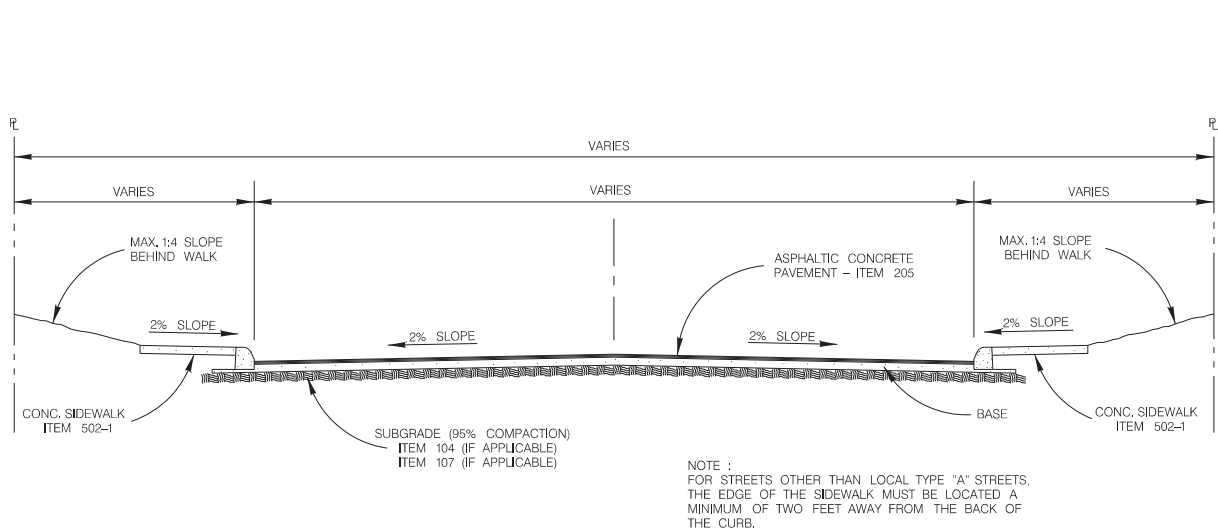
HANDRAIL FOR CONCRETE STEPS
ITEM 522

MAY 2009

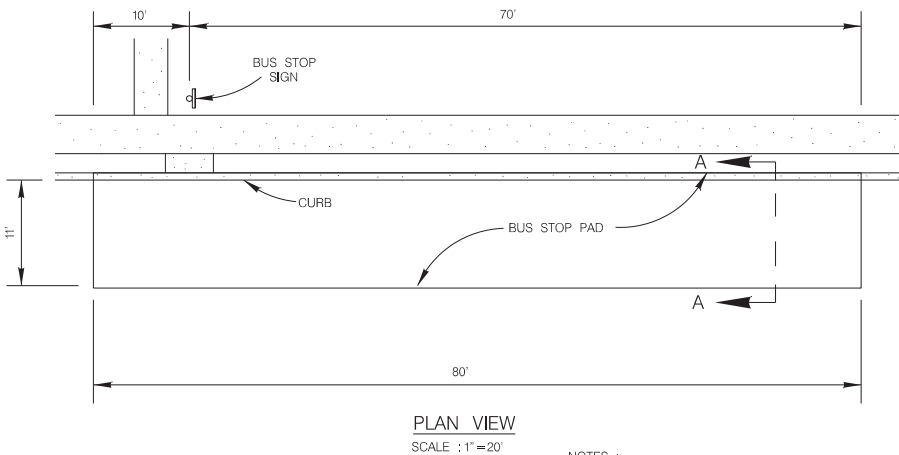
CITY OF SAN ANTONIO
CAPITAL IMPROVEMENTS MANAGEMENT SERVICES DEPARTMENT

**MISCELLANEOUS
CONSTRUCTION STANDARDS I**

DRWN. BY: V. VASQUEZ	PROJECT NO.:	DATE:
DSGN. BY:	CHKD. BY: R.S. HOSSEINI, P.E.	SHEET NO.: 11 OF 25

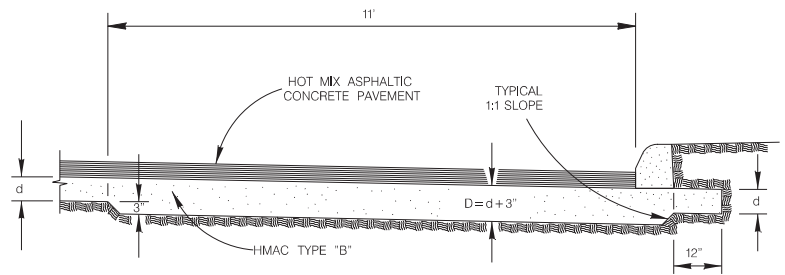


TYPICAL STREET SECTION
SCALE : 1"=8'

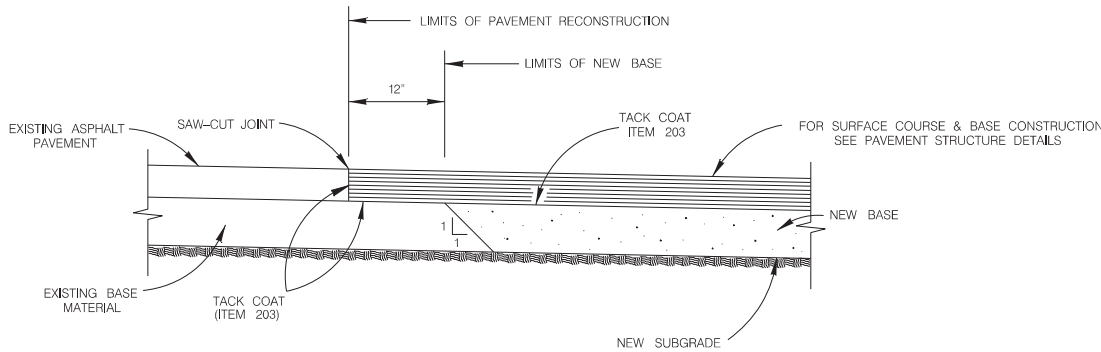


PLAN VIEW
SCALE : 1"=20'

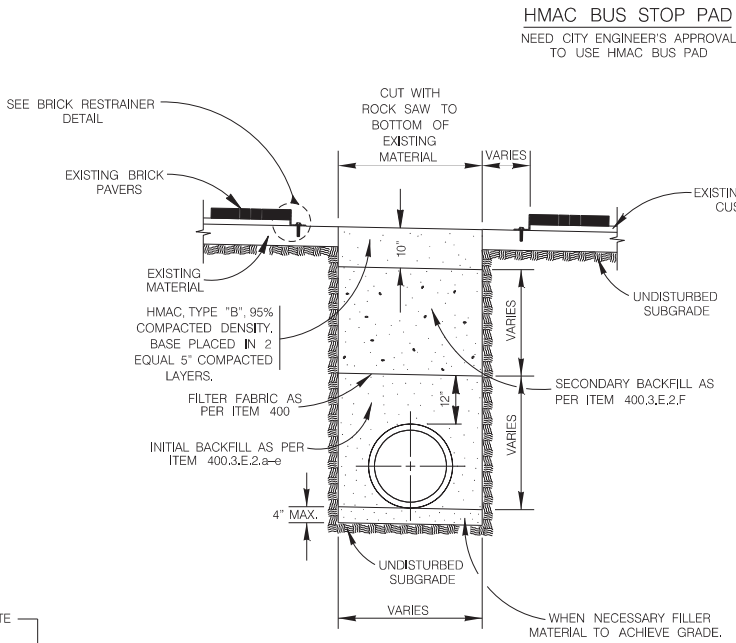
- NOTES :
- EXCAVATION FOR THICKENED PAVEMENT SECTION WILL BE PAID UNDER ITEM NO.104 "STREET EXCAVATION".
 - BASE MATERIALS :
A) IF THE MEASUREMENT FOR THE HMAc MATERIAL IS PER TON, THICKENED PAVEMENT SECTION WILL BE PAID FOR UNDER ITEM NO.205, TYPE "B" - PER TON.
B) IF THE MEASUREMENT FOR THE HMAc MATERIAL IS PER SQUARE YARD, NO EXTRA PAYMENT WILL BE MADE FOR THE THICKENED PAVEMENT.



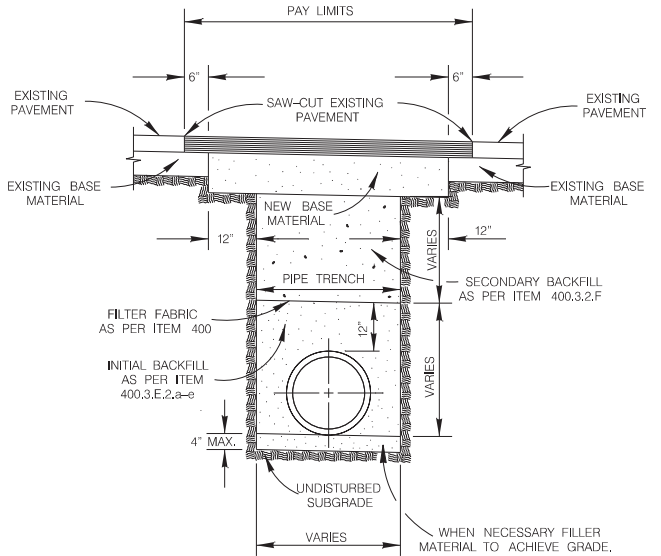
SECTION "A-A"
SCALE : 1"=4'



PAVEMENT JUNCTION DETAILS
SCALE : 1"=2'

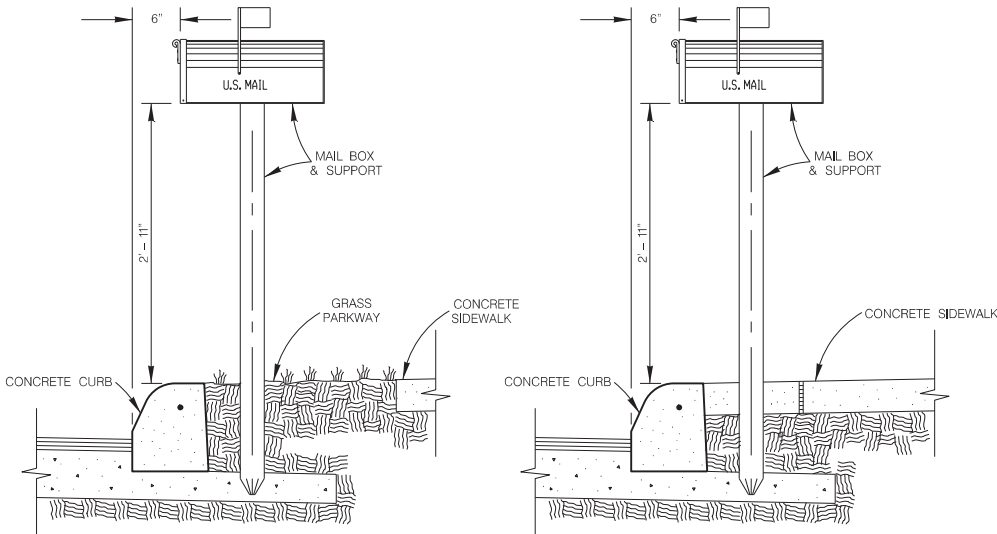


TYPICAL BASE REPLACEMENT FOR
BRICK SURFACED STREET SECTION
ITEM 511.3
SCALE : 1"=4'



- NOTES :
- FOR LOCAL TYPE "A" & "B" STREETS (RESIDENTIAL) USE 6" ASPHALT CONCRETE BASE TYPE "B" WITH 1-1/2" TYPE "D" HOT MIX ASPHALTIC CONCRETE PAVEMENT.
 - FOR ARTERIAL & SECONDARY STREETS (COMMERCIAL) USE 12.5" TYPE "B" HOT MIX ASPHALTIC CONCRETE PAVEMENT LEVELING-UP COURSE & 1-1/2" TYPE "D" HOT MIX ASPHALTIC CONCRETE PAVEMENT SURFACE COURSE.

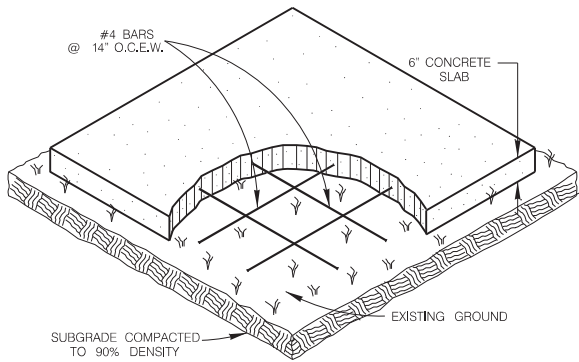
TYPICAL PAVEMENT REPLACEMENT
ITEM 511
SCALE : 1"=4'



MAIL BOX PERPENDICULAR TO CURB
WHEN SIDEWALK IS DETACHED FROM CURB
SCALE : 1"=2'

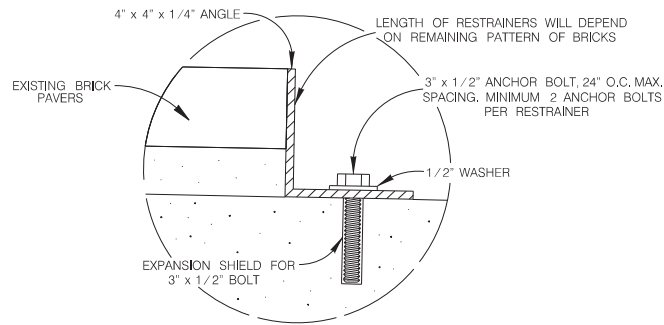
MAIL BOX PERPENDICULAR TO CURB
WHEN SIDEWALK IS ATTACHED TO CURB
SCALE : 1"=2'

MAIL BOX LOCATION
ITEM 513.1



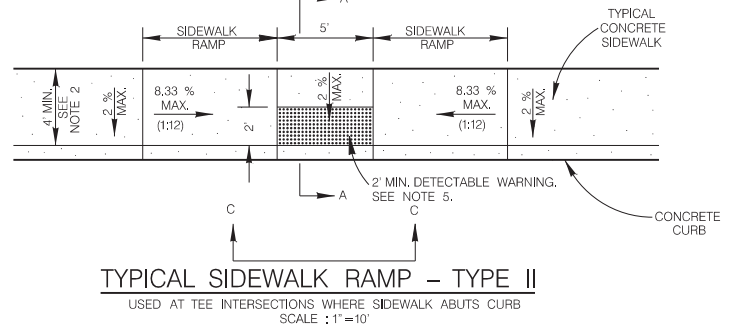
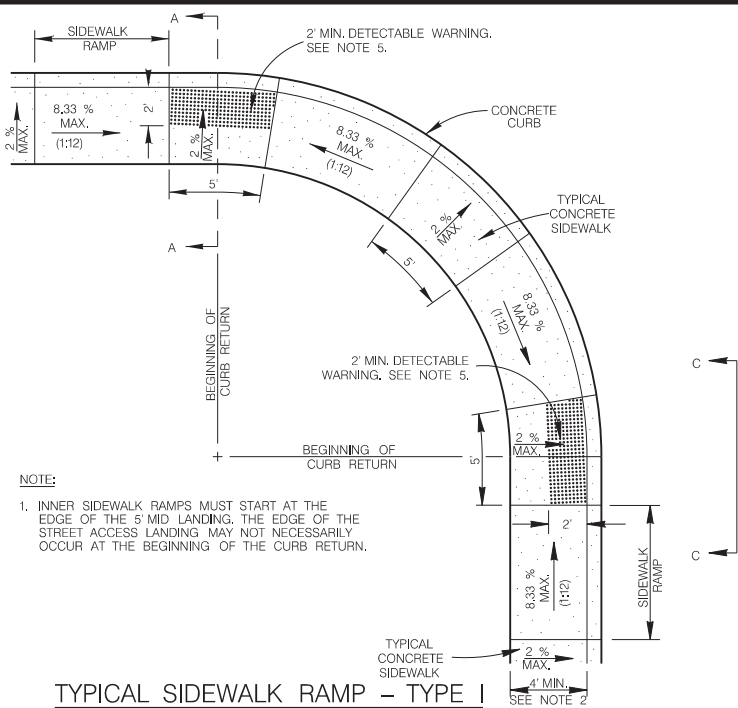
- MAIL BOX PAD NOTES :
- THE CONTRACTOR WILL CONSTRUCT SLABS FOR "TEMPORARY MAIL BOX COLLECTION PAD" FOR THE UNITED STATES POSTAL SERVICE WITH LOCATIONS AND SIZES SPECIFIED BY THE CITY ENGINEER DURING CONSTRUCTION.
 - THE CONSTRUCTION OF SLABS SHALL CONFORM TO ITEM 513 "REMOVING AND RELOCATING MAILBOXES".
 - PAYMENT WILL BE MADE UNDER ITEM 513.2 "COMMUNITY MAILBOX SLAB - PER SQUARE YARD".
 - UNIT PRICE WILL INCLUDE REMOVAL OF "TEMPORARY MAIL BOX COLLECTION PAD" SLABS AT THE END OF THE PROJECT, NO SEPARATE PAY ITEM.

COMMUNITY
MAIL BOX SLAB
ITEM 513.2
SCALE : 1"=4'



BRICK RESTRAINER DETAIL
SCALE : 1:6

FEBRUARY 2010		
CITY OF SAN ANTONIO		
CAPITAL IMPROVEMENTS MANAGEMENT SERVICES DEPARTMENT		
MISCELLANEOUS CONSTRUCTION STANDARDS II		
% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: V. VASQUEZ	DSGN. BY:	CHKD. BY: R.S. HOSSEINI, P.E.
		SHEET NO.: 12 OF 25



GENERAL NOTES

- WHEN POSSIBLE SIDEWALKS SHOULD BE PLACED NEXT TO THE PROPERTY LINE, ALLOWING A MINIMUM OF 1 FOOT BUFFER. DEVIATION OF THE PATHWAY FROM A STRAIGHT LINE IS ENCOURAGED TO AVOID TREES OR OTHER OBSTRUCTIONS.
- FOR LOCAL TYPE "A" STREETS, SIDEWALKS SHALL HAVE A MINIMUM UNOBSTRUCTED WIDTH OF 4' AND IF SEPARATED FROM THE CURB, THE SIDEWALK SHALL BE LOCATED A MINIMUM OF 2' FROM THE BACK OF CURB.
- FOR OTHER THAN LOCAL TYPE "A" STREETS, SIDEWALKS SHALL HAVE A MINIMUM UNOBSTRUCTED WIDTH OF 4' AND SEPARATED A MINIMUM OF 2' FROM THE BACK OF CURB OR AS AN OPTION, THE SIDEWALK SHALL HAVE A MINIMUM WIDTH OF 6' WHEN LOCATED AT THE BACK OF CURB.
- SIDEWALK RAMP LENGTHS PRESENTED IN TABLE 1 ARE GUIDELINES ONLY. SIDEWALK RAMP LENGTHS SHALL BE OF SUFFICIENT LENGTH TO MAINTAIN 8.33% (1:12) MAXIMUM SLOPE.
- ALL CURB-RAMPS OR LANDINGS ABUTTING THE CROSSWALK SHALL HAVE A DETECTABLE WARNING 24 INCHES DEEP (IN THE DIRECTION OF PEDESTRIAN TRAVEL) AND EXTENDING THE FULL WIDTH OF THE CURB RAMP OR LANDING. THE DETECTABLE WARNING SHALL CONSIST OF RAISED TRUNCATED DOMES, ALIGNED IN A GRID PATTERN WITH A DIAMETER OF 0.9 INCHES (23 MM), A HEIGHT OF NOMINAL 0.2 INCHES (5 MM) AND A CENTER-TO-CENTER SPACING OF NOMINAL 2.35 INCHES (60 MM). THE DETECTABLE WARNING SURFACE SHALL BE A CAST-IN-PLACE TILE CONFORMING TO THE CITY OF SAN ANTONIO STANDARD SPECIFICATIONS OR PAVERS CONFORMING TO TxDOT STANDARD PED-05, PEDESTRIAN FACILITIES.
- DETECTABLE WARNINGS SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT. THE MATERIAL USED TO PROVIDE CONTRAST SHALL BE AN INTEGRAL PART OF THE WALKING SURFACE.
- SIDEWALK RAMP TYPE V SHALL BE USED ONLY WHERE THERE IS SIGNIFICANT RESTRICTION WITHIN THE PARKWAY TO CONSTRUCT TYPE I OR TYPE II RAMPS.
- CONSTRUCTION OF ALL WHEELCHAIR RAMPS TO BE INCLUDED UNDER ITEMS "500 - CONCRETE CURB, GUTTER, AND CONCRETE CURB AND GUTTER" AND "/OR "502 - CONCRETE SIDEWALKS". RAMP SURFACE SHALL BE BRUSH FINISHED.
- THESE DETAILS ARE FOR REFERENCE ONLY. ACTUAL LOCATIONS OF WHEELCHAIR RAMPS TO BE SHOWN ON CONSTRUCTION PLANS. CITY CONSTRUCTION INSPECTOR CAN ADJUST LOCATIONS FOR SAFETY OR UTILITY CLEARANCE.
- SIDEWALKS LESS THAN 5 FEET IN WIDTH SHALL BE PROVIDED WITH A PASSING SPACE AT A MAXIMUM SPACING OF 200 FEET.
- WHEELCHAIR RAMP SHALL BE CONSTRUCTED WITH 4" CLASS "A" CONCRETE AND 2" MINIMUM GRAVEL, CRUSHED ROCK OR FLEXIBLE BASE MATERIAL.
- REINFORCING STEEL SHALL BE #3 BARS AT 18" O.C.E.W. OR 6" x 6" - W2.9 x W2.9 WIRE MESH.
- SIDEWALK GRADES SHALL NOT EXCEED THE GRADE ESTABLISHED FOR THE ADJACENT ROADWAY. ANY SIDEWALK CONSTRUCTION THAT DEVIATES FROM THE NATURAL GRADE OF THE ROADWAY TO CREATE A GRADE STEEPER THAN THE EXISTING ROADWAY WILL REQUIRE RAMPS, HANDRAILS AND RESTING PLATFORMS TO BE CONSTRUCTED IN ACCORDANCE WITH ADA AND TAS STANDARDS.
- SIDEWALK CROSS GRADE SHALL HAVE A MAXIMUM SLOPE OF 2%. LANDINGS SHALL HAVE A MAXIMUM SLOPE OF 2% IN ANY DIRECTION.
- THE CHANGE OF GRADE BETWEEN ADJACENT SURFACES SHALL BE LESS THAN 11%. THE CHANGE OF GRADE SHALL BE DEFINED AS THE ALGEBRAIC DIFFERENCE OF THE ADJACENT SURFACE SLOPES. IN THE CASE OF A STREET ACCESS RAMP DESIGNED AT THE 8.33% MAXIMUM SLOPE, THE ADJACENT PAVEMENT CROSS SLOPE SHALL BE LESS THAN 2.67% (I.E., 8.33--2.67=11). IN ADDITION, THE ADJACENT PAVEMENT CROSS SLOPE SHALL BE LESS THAN OR EQUAL TO 5%.
- IF THE CHANGE OF GRADE BETWEEN ADJACENT SURFACES IS GREATER THAN OR EQUAL TO 11%, A LEVELING STRIP, 2 FEET IN LENGTH, SHALL BE PROVIDED TO TRANSITION THE ADJACENT SURFACES.
- ADA COMPLIANCE IN ALTERATIONS INCLUDE ONLY THAT WORK WITHIN THE LIMITS, BOUNDARIES OR SCOPE OF A PLANNED PROJECT.

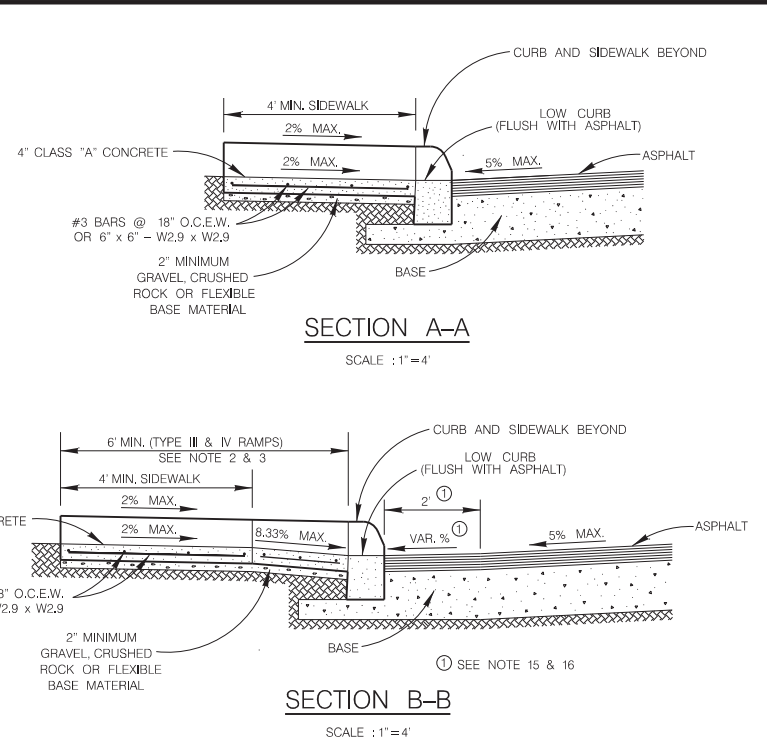
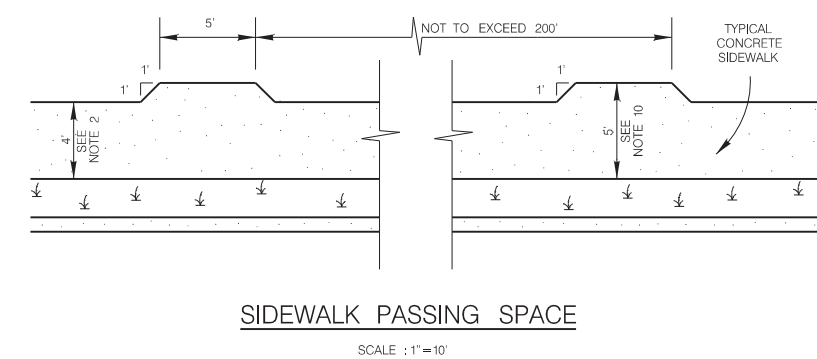
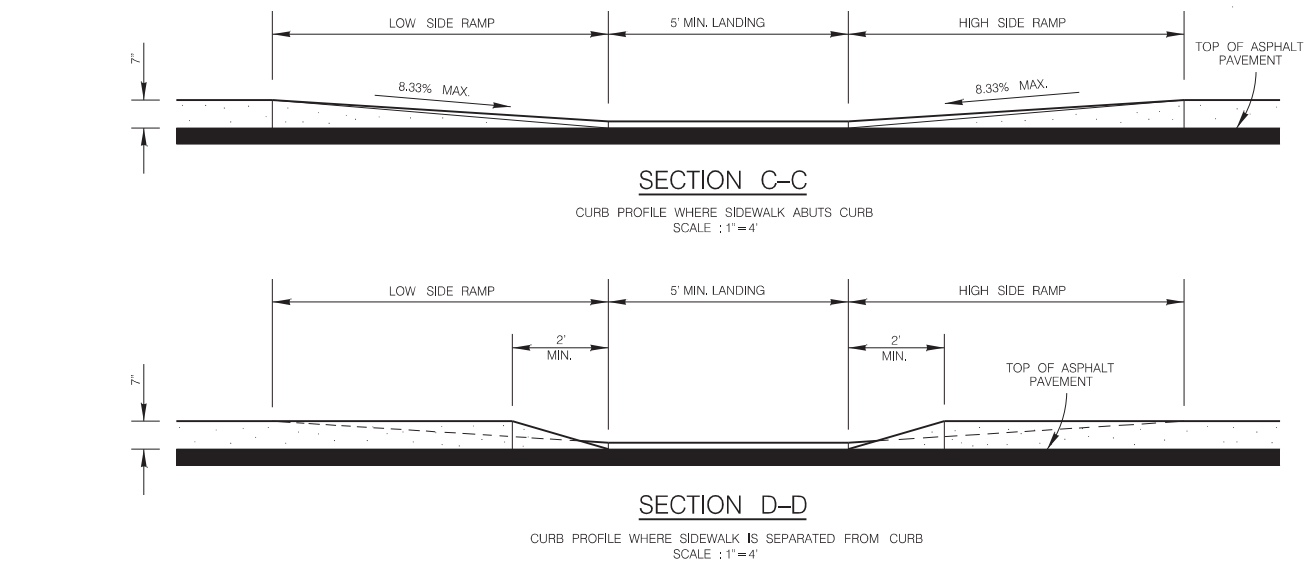
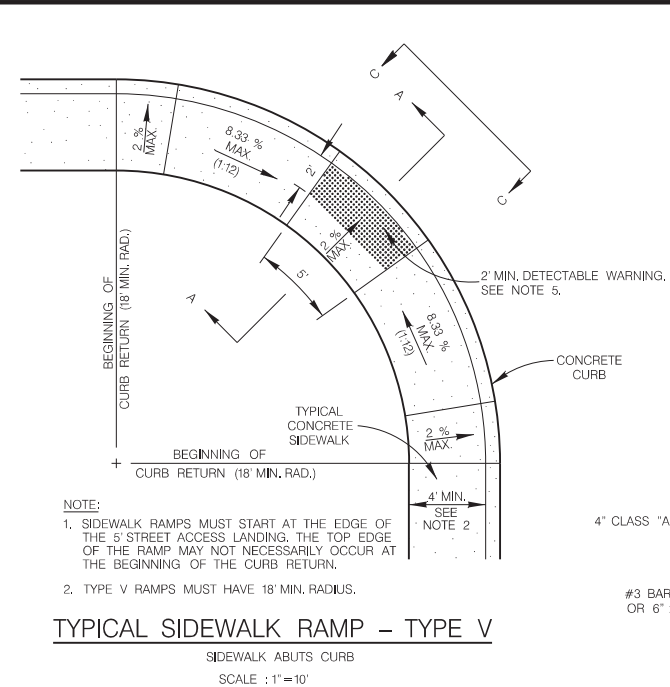
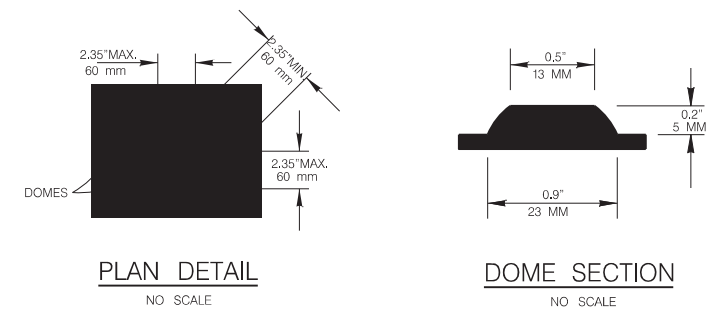
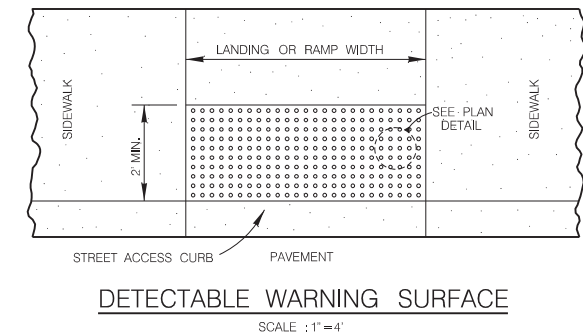
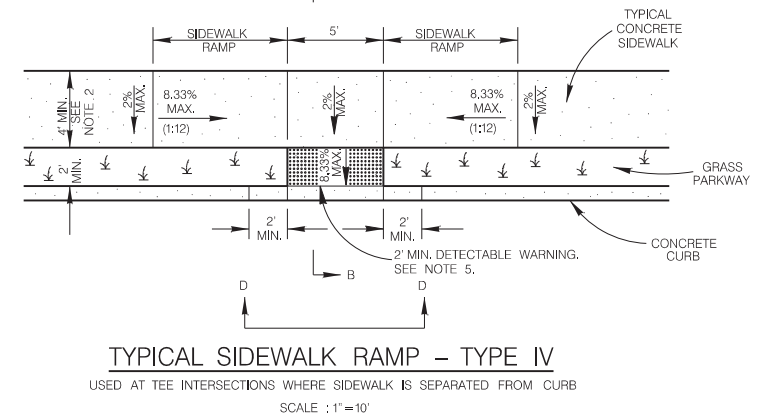
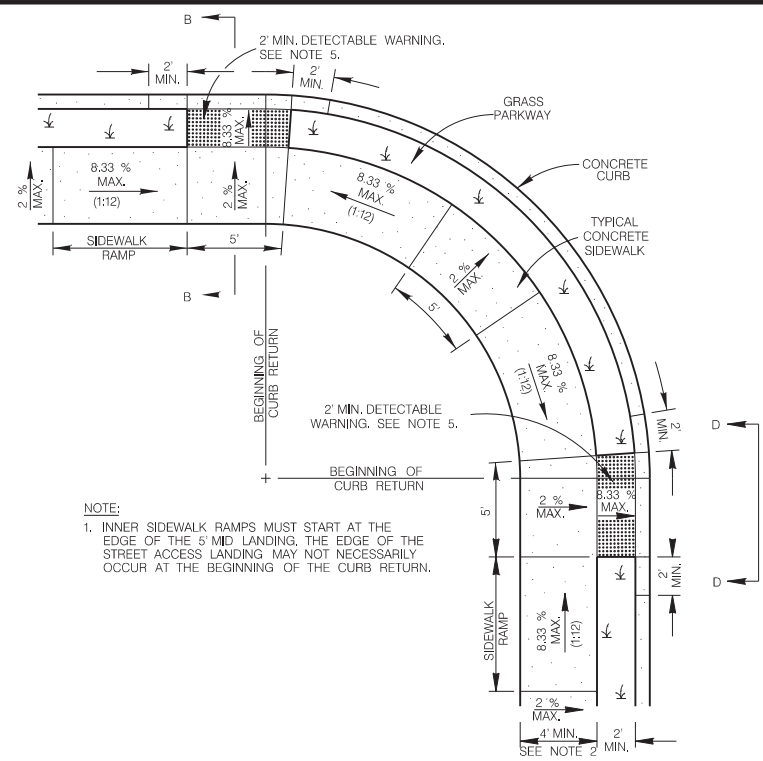


TABLE 1 (SEE NOTE 4)		
GUTTER SLOPE	SIDEWALK RAMP LENGTH (1:12)	
	LOW SIDE	HIGH SIDE
1%	5'-6"	7'-2"
2%	5'-0"	8'-4"
3%	4'-6"	10'-0"
4%	4'-2"	12'-6"
5%	3'-10"	16'-8"

MAY 2009

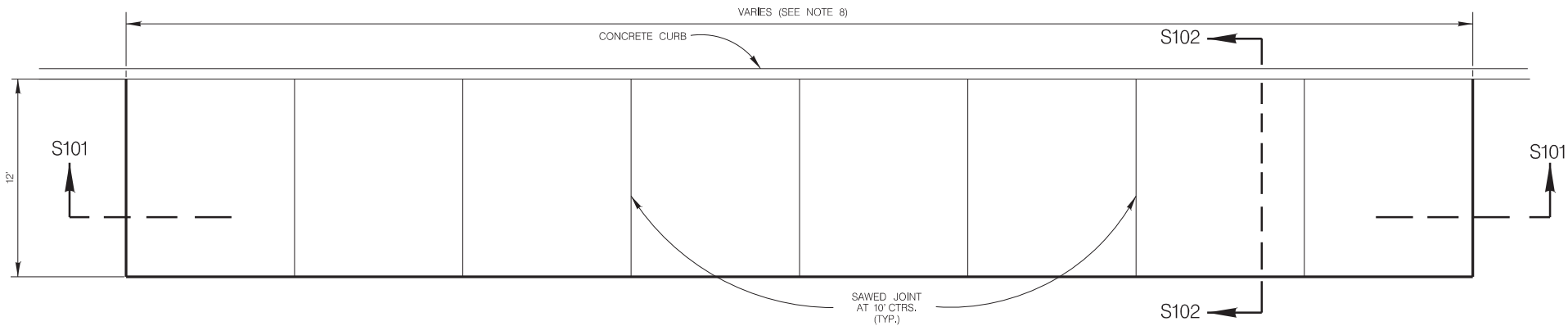
CITY OF SAN ANTONIO
CAPITAL IMPROVEMENTS MANAGEMENT SERVICES DEPARTMENT

WHEELCHAIR RAMP STANDARDS

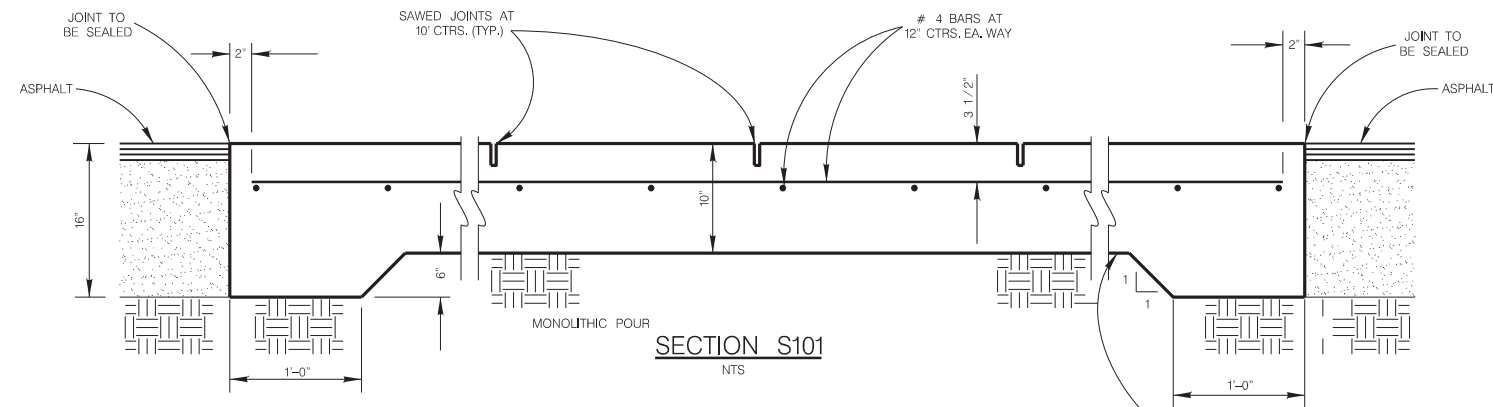
% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: V. VAQUEZ	DSGN. BY:	CHKD. BY: R.S. HOSSEINI, P.E.

SHEET NO.: 13 OF 25

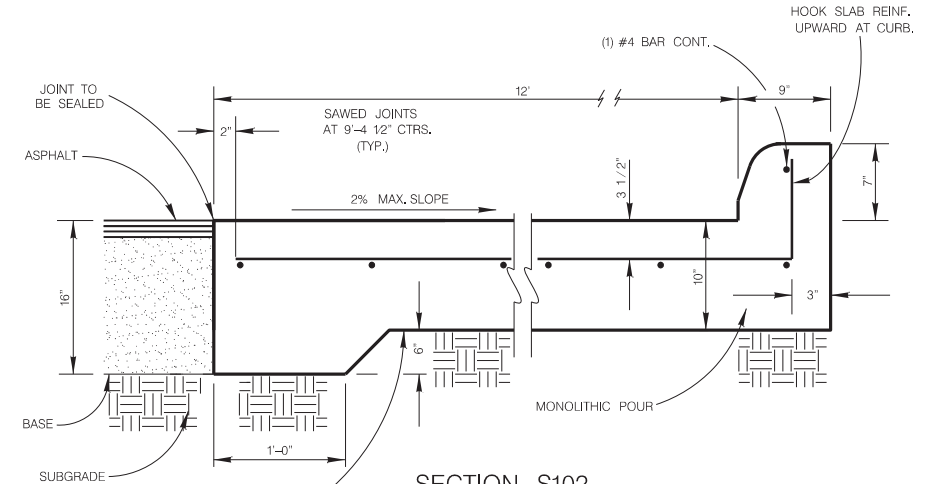
NOTE:
STAMPED CONCRETE TRUNCATED DOMES WILL NOT BE ALLOWED TO BE USED FOR DETECTABLE WARNING ON WHEELCHAIR RAMPS. CONTRACTOR MUST SUBMIT TRUNCATED DOME INFORMATION THAT IS TO BE USED ON WHEELCHAIR RAMPS TO THE PROJECT MANAGER FOR APPROVAL AT LEAST 30 DAYS PRIOR TO INSTALLATION.



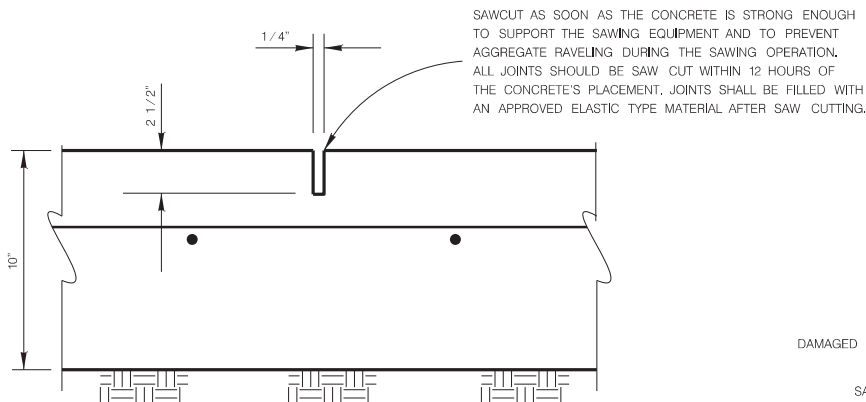
PLAN VIEW
NTS



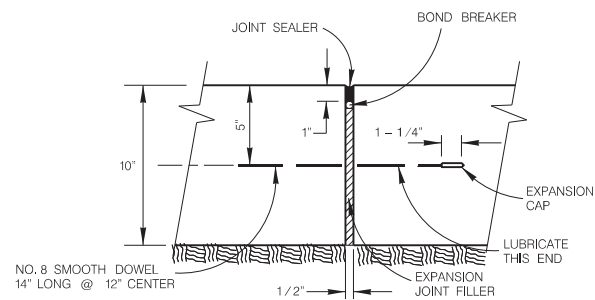
SECTION S101
NTS



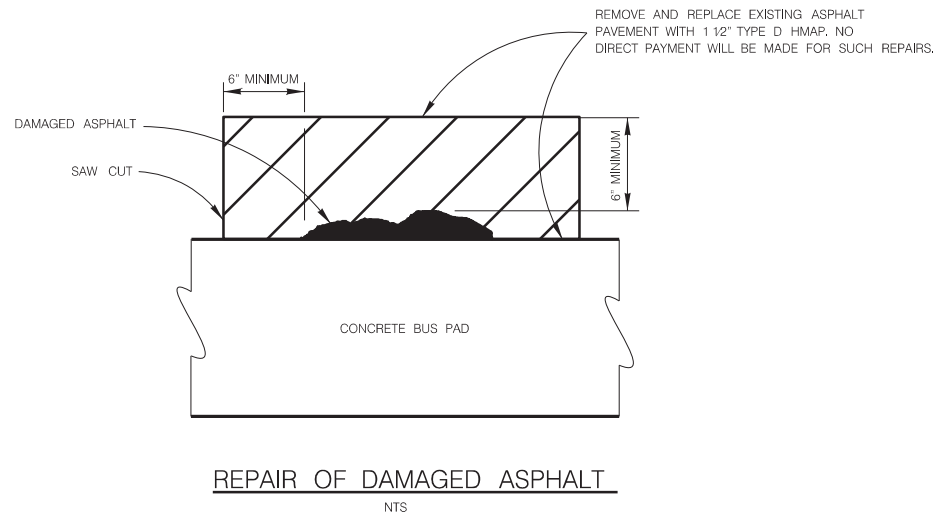
SECTION S102
NTS



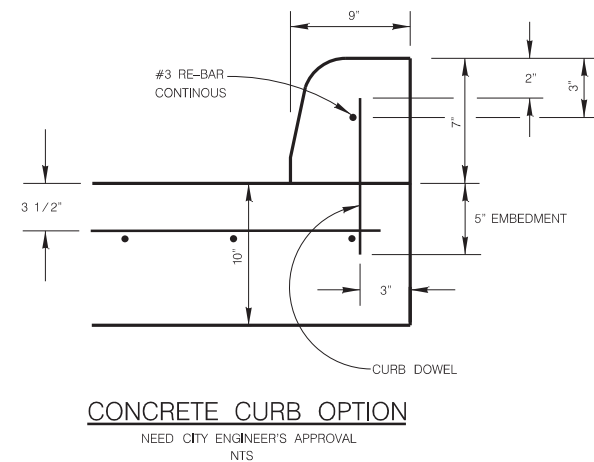
SAWED JOINT DETAIL
NTS



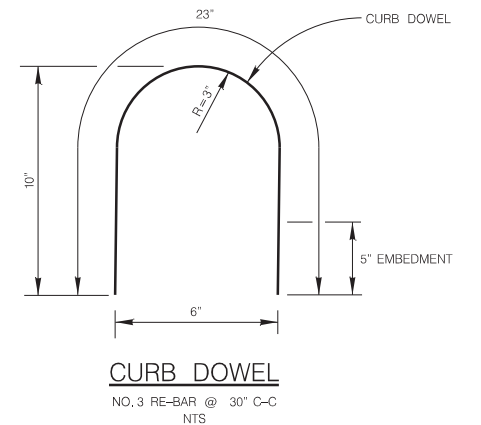
EXPANSION JOINT DETAIL
SCALE : 1" = 1'



REPAIR OF DAMAGED ASPHALT
NTS



CONCRETE CURB OPTION
NEED CITY ENGINEER'S APPROVAL
NTS



CURB DOWEL
NO. 3 RE-BAR @ 30" C-C
NTS

GENERAL NOTES

1. ALL CONCRETE SHALL TEST 4,000 P.S.I. AT 28 DAYS.
2. BUS STOP CONCRETE PAD CONSTRUCTION SHALL BE PAID UNDER ITEM 209 AT THE UNIT PRICE BID, WHICH PRICE SHALL BE FULL COMPENSATION FOR ALL DEMOLITION, REMOVAL OF EXISTING CURB, EXCAVATION, HAULING, CRUSHED LESTONE, REINFORCING STEEL, CONCRETE, CONCRETE CURB, JOINTS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.
3. BUS PAD AND CURB SHALL BE MONOLITHICALLY POURED. ALL EXISTING CURBING SHALL BE REMOVED AND REPLACED AS PER STANDARD DETAILS.
4. THE CONTRACTOR SHALL CONSTRUCT AN EXPANSION JOINT MIDWAY IF THE "CONCRETE BUS STOP PAD" IS LONGER THAN 150 FEET. NO DIRECT PAYMENT SHALL BE MADE FOR CONSTRUCTION OF AN EXPANSION JOINT.
5. ACTUAL BUS PAD LENGTH AND WIDTH TO BE FIELD DETERMINED BY CITY ENGINEER OR HIS DESIGNATED REPRESENTATIVE.
6. DO NOT DRIVE ON PAD UNTIL CONCRETE HAS REACHED A STRENGTH OF 2,800 P.S.I.
7. BREAK TEST CYLINDERS AS FOLLOWS:
2 AT 3 DAYS
2 AT 7 DAYS
2 AT 28 DAYS
8. CONCRETE BUS PAD LENGTH (OR AS SHOWN ON THE PLANS):
30 M.P.H. - 100'
35 M.P.H. - 160'
40 M.P.H. - 160'
45 M.P.H. - 200'

NOTE:
THE CONTRACTOR SHALL CONSTRUCT AN EXPANSION JOINT MIDWAY IF THE "CONCRETE BUS STOP PAD" IS LONGER THAN 150 FEET. NO DIRECT PAYMENT SHALL BE MADE FOR CONSTRUCTION OF AN EXPANSION JOINT.

MAY 2009

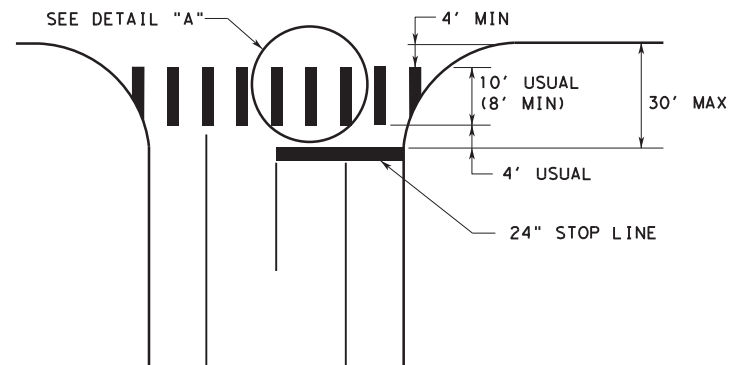
CITY OF SAN ANTONIO
CAPITAL IMPROVEMENTS MANAGEMENT SERVICES DEPARTMENT

CONCRETE
BUS STOP PAD

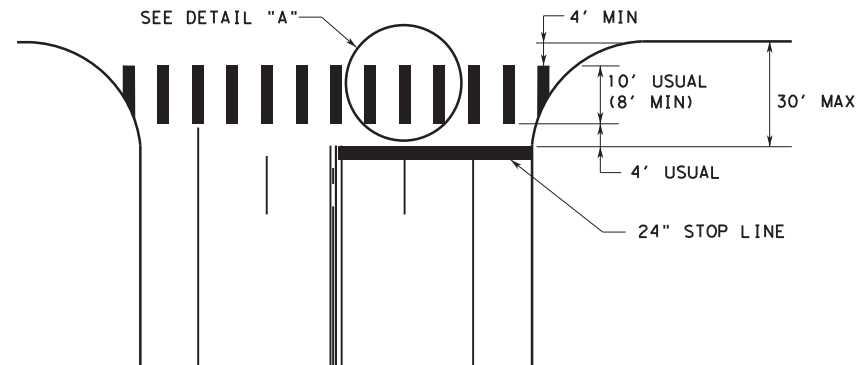
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DRWN. BY: V. VASQUEZ	DSGN. BY: L. MALTOS	CHKD. BY: R.S. HOSSEINI, P.E.

SHEET NO.: 14 OF 25

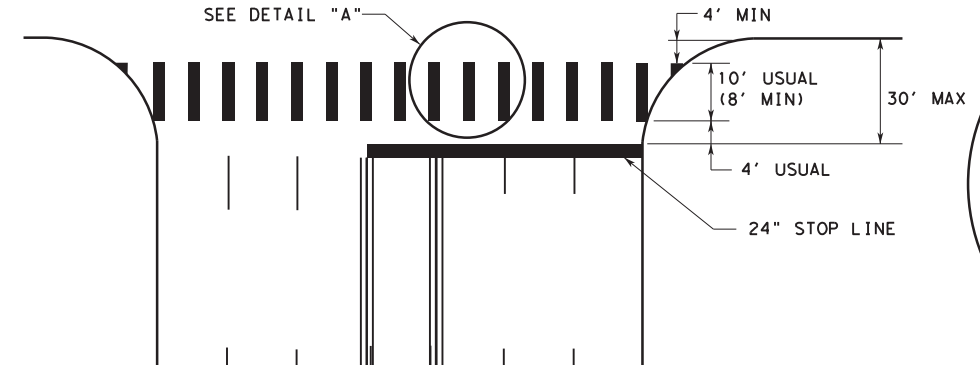
TWO LANES WITH SHOULDERS



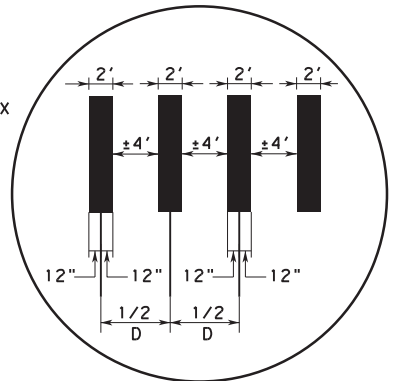
FOUR LANES WITH SHOULDERS



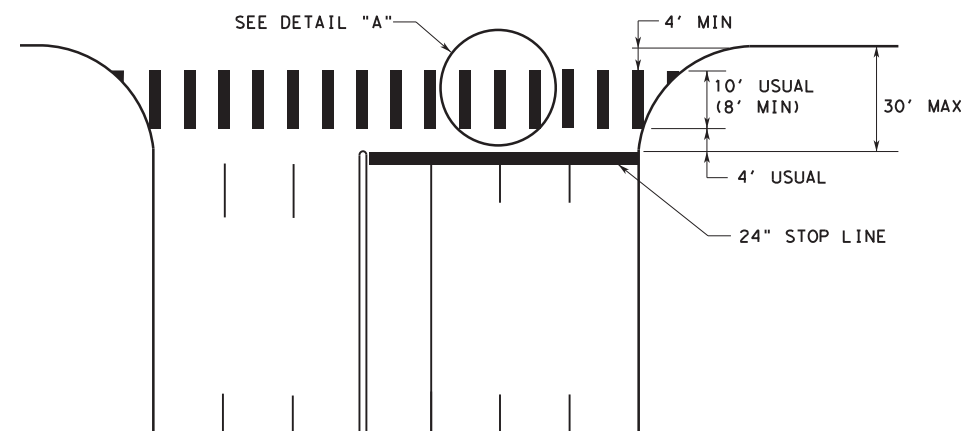
MULTI - LANES



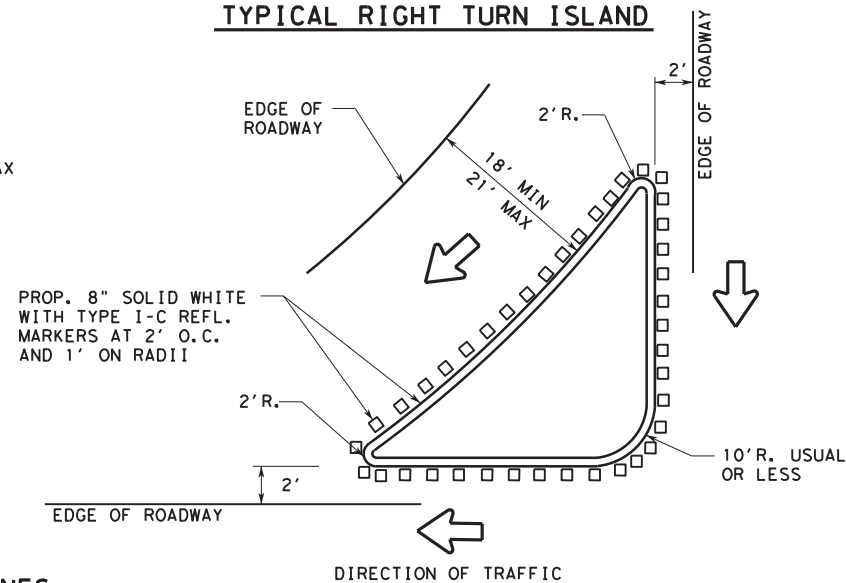
DETAIL "A"



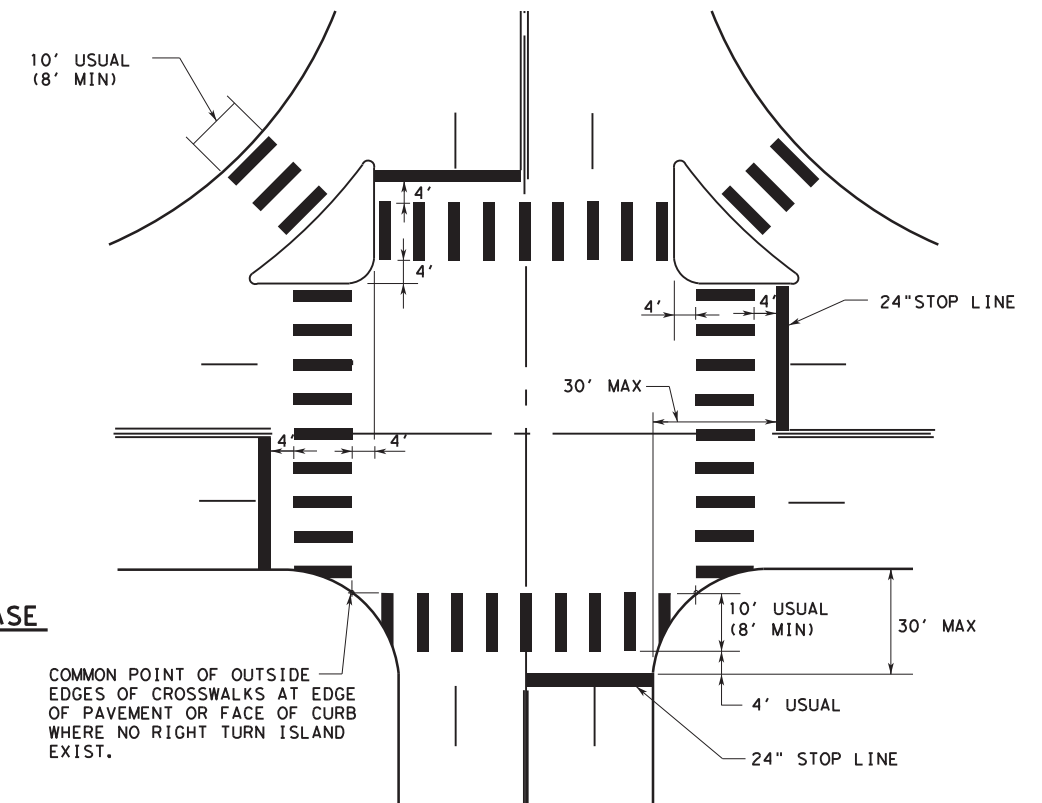
MULTI - LANE WITH MEDIAN



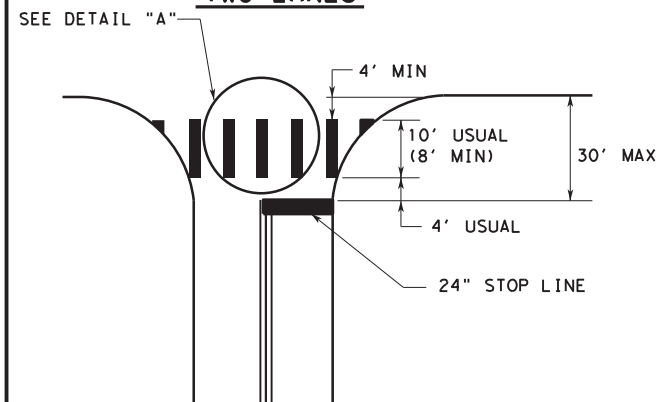
TYPICAL RIGHT TURN ISLAND



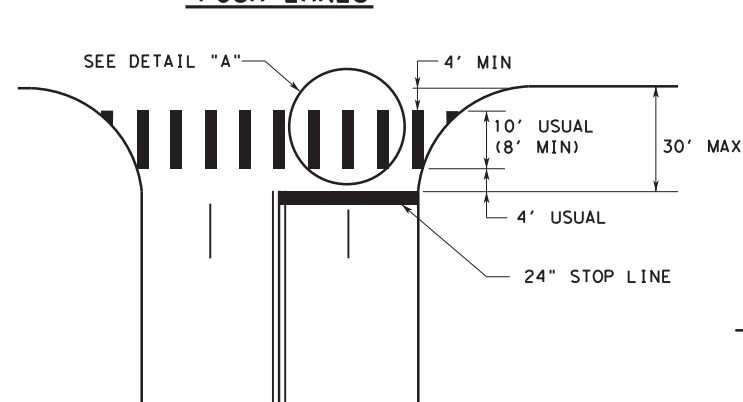
INTERSECTION WITH RIGHT - TURN ISLANDS



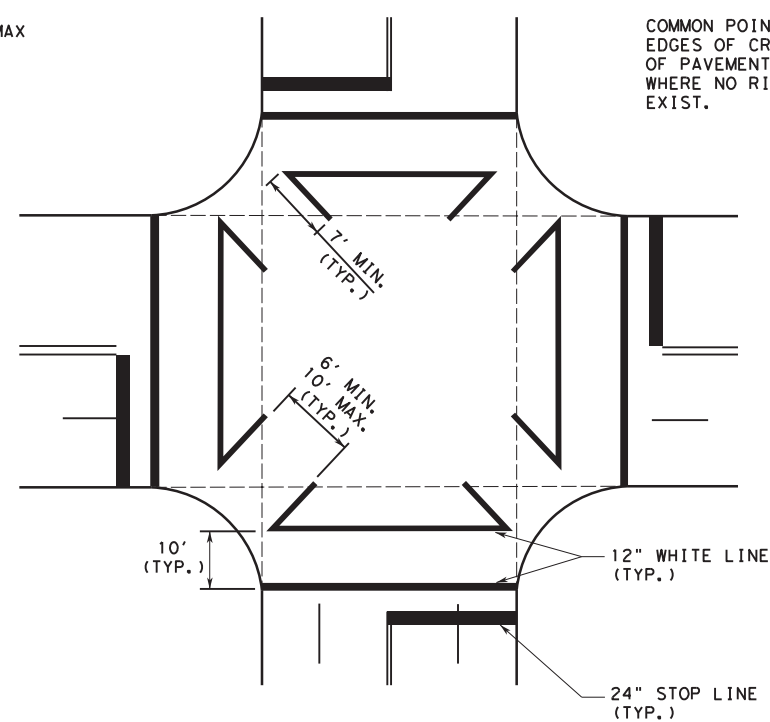
TWO LANES



FOUR LANES

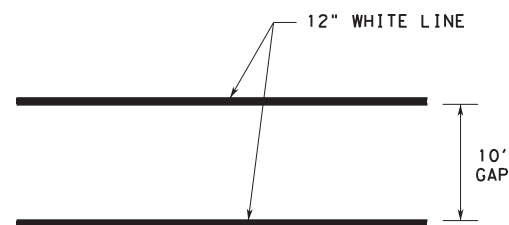


EXCLUSIVE PEDESTRIAN PHASE



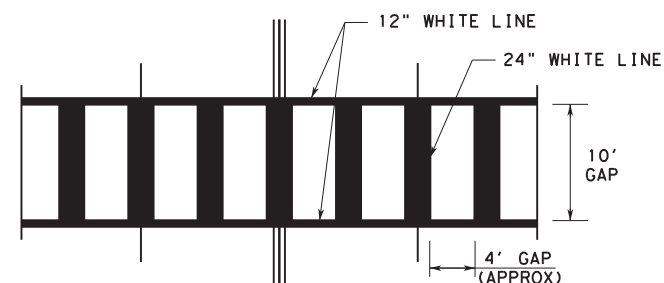
COMMON POINT OF OUTSIDE EDGES OF CROSSWALKS AT EDGE OF PAVEMENT OR FACE OF CURB WHERE NO RIGHT TURN ISLAND EXIST.

CENTRAL BUSINESS DISTRICT CROSSWALK DETAIL



HIGH VISIBILITY CROSSWALK DETAIL

TYPICALLY USED AT SIGNALIZED AND NON-SIGNALIZED MID-BLOCK CROSSINGS FOR COLLECTOR AND ARTERIAL ROADWAYS AND AT LOCATIONS REQUIRING EXTRA EMPHASIS.



NOTES:

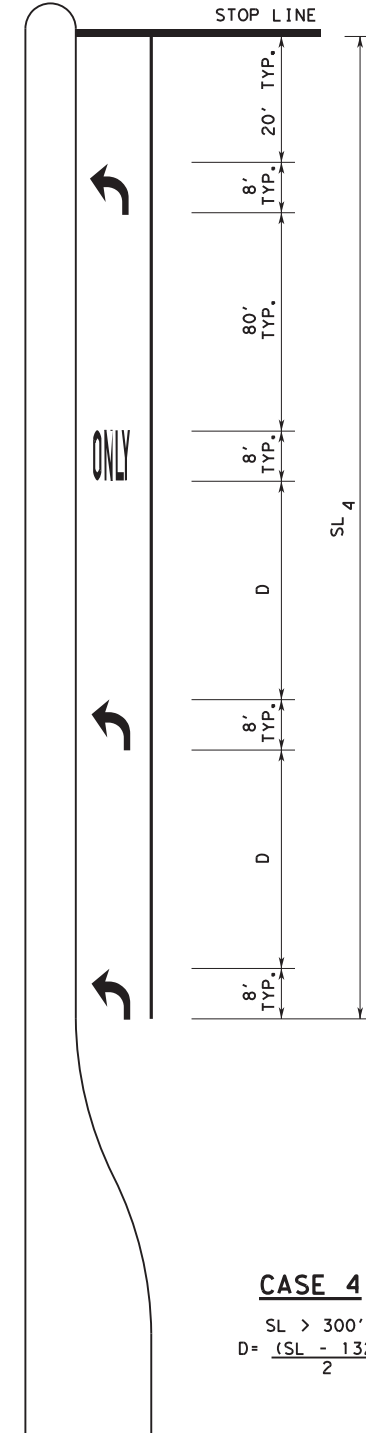
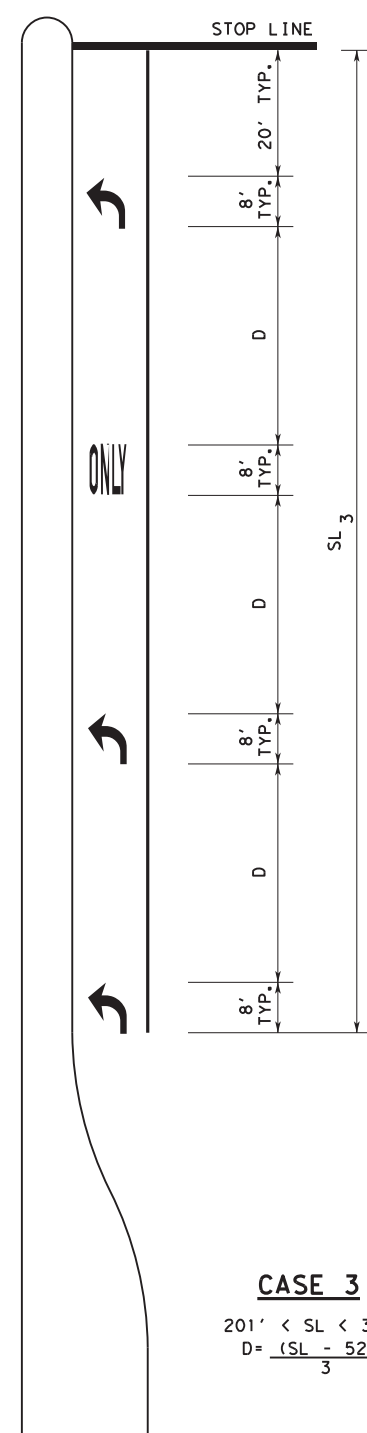
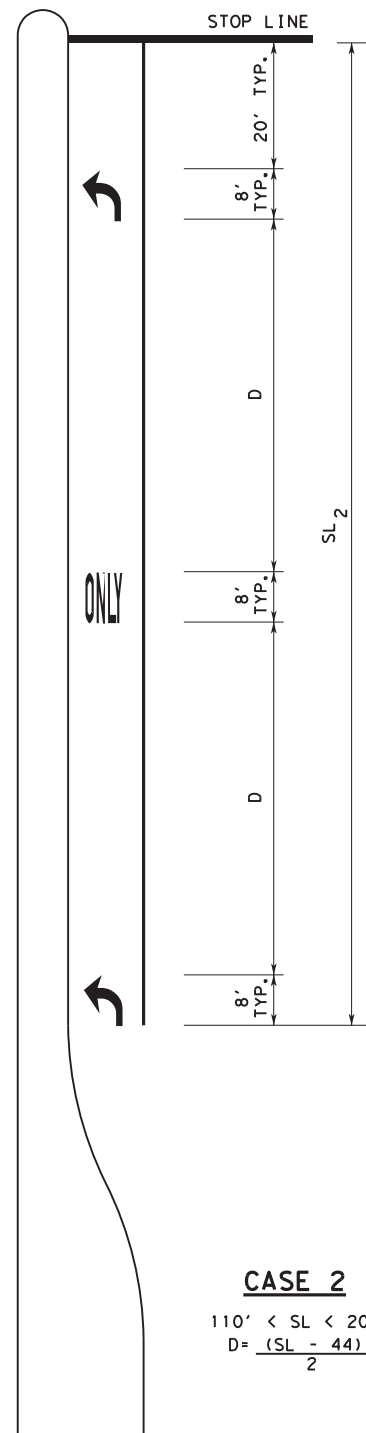
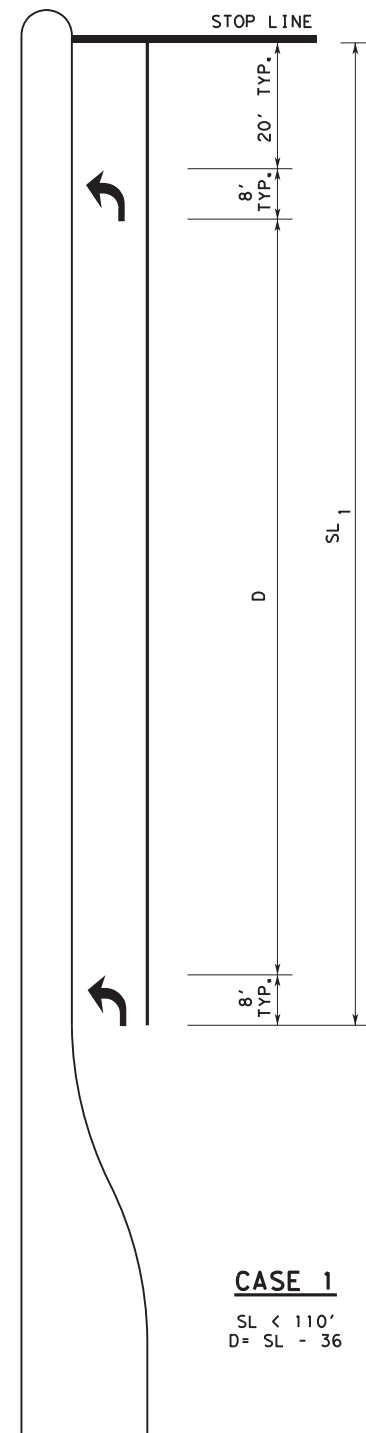
1. CROSSWALKS AND STOP LINES SHALL BE WHITE.
2. "D" IS EQUAL TO ONE HALF THE WIDTH OF TRAVEL LANE.

SEPTEMBER 2009

CITY OF SAN ANTONIO
DEPARTMENT OF PUBLIC WORKS

TRAFFIC ENGINEERING STANDARDS
TYPICAL CROSSWALK
DETAILS
SHEET 9 OF 16

% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: LAN	DSGN. BY: C.B.V.	CHKD. BY: M.E.
		SHEET NO.: 15 OF 25



KEY:

SL - STORAGE LENGTH (FEET)

D - DISTANCE BETWEEN ARROWS AND LEGENDS (FEET)

GENERAL NOTES:

1. THESE DETAILS ALSO APPLY TO RIGHT-TURN LANES.
2. FOR DUAL-TURN LANES, DIMENSIONS SHALL BE THE SAME FOR EACH LANE.
3. SL DIMENSION IS FROM STOP LINE TO END OF TURN LANE, WHICH DOES NOT INCLUDE TAPER LENGTH.
4. PAVEMENT ARROWS AND "ONLY" LEGEND MARKINGS ARE TYPICALLY USED AT SIGNALIZED INTERSECTIONS AND AT UNSIGNALIZED INTERSECTIONS WHERE A DEMONSTRATED NEED EXISTS.
5. MINIMUM SL = 110'. SL MAY BE LESS THAN 110 FEET AS DIRECTED BY THE CITY TRAFFIC ENGINEER.

SEPTEMBER 2009

CITY OF SAN ANTONIO

DEPARTMENT OF PUBLIC WORKS

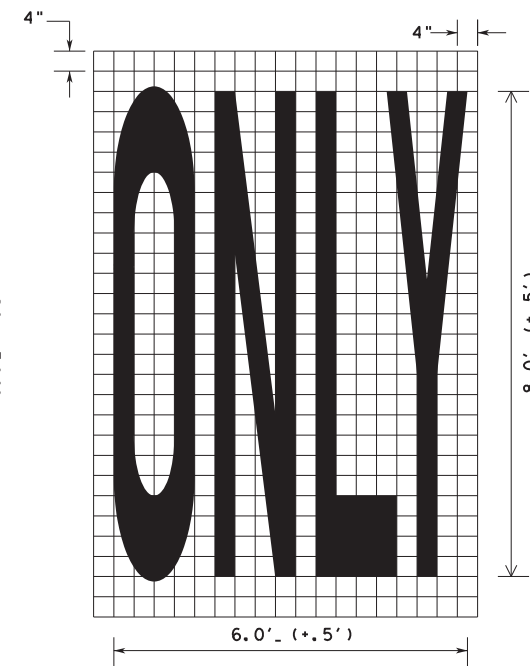
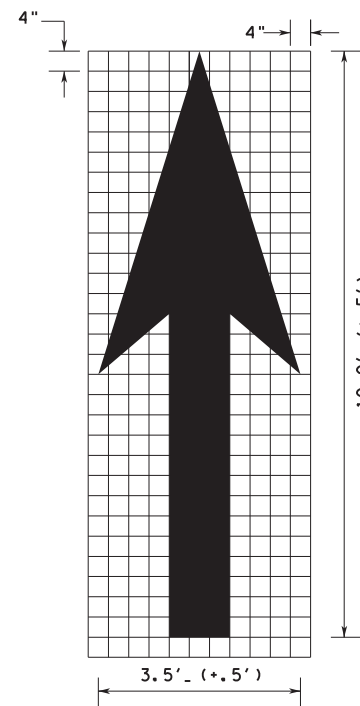
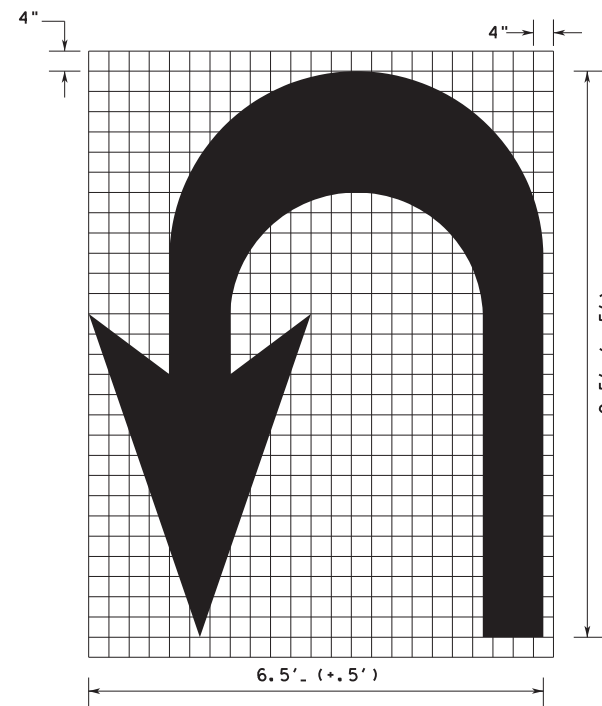
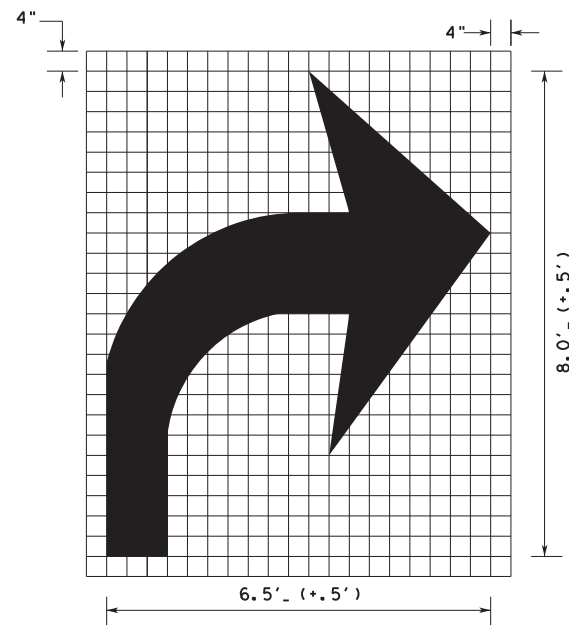
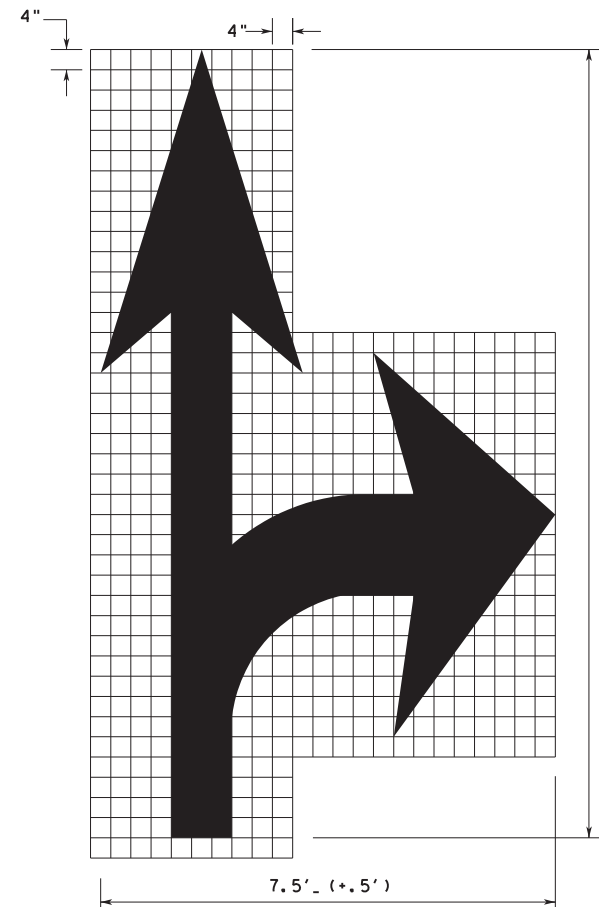
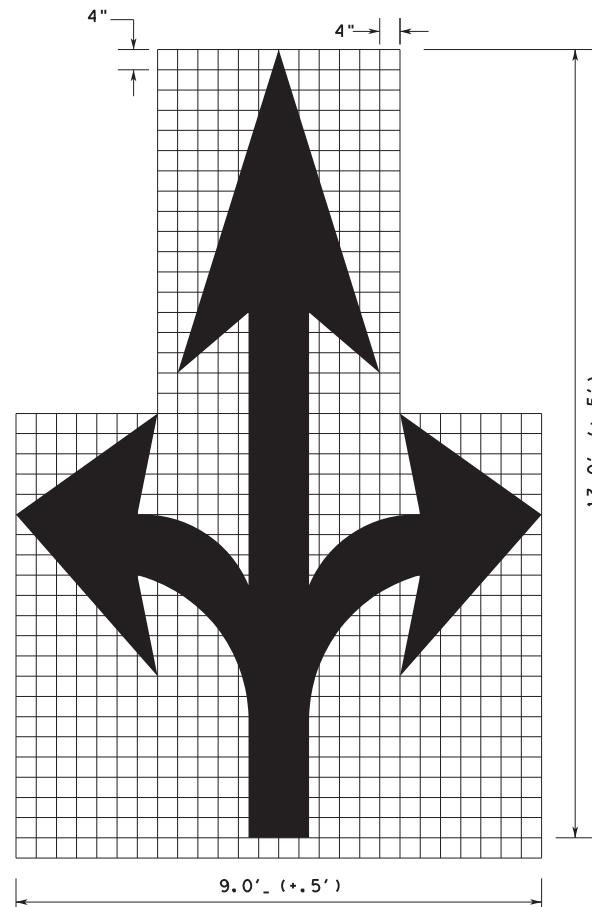
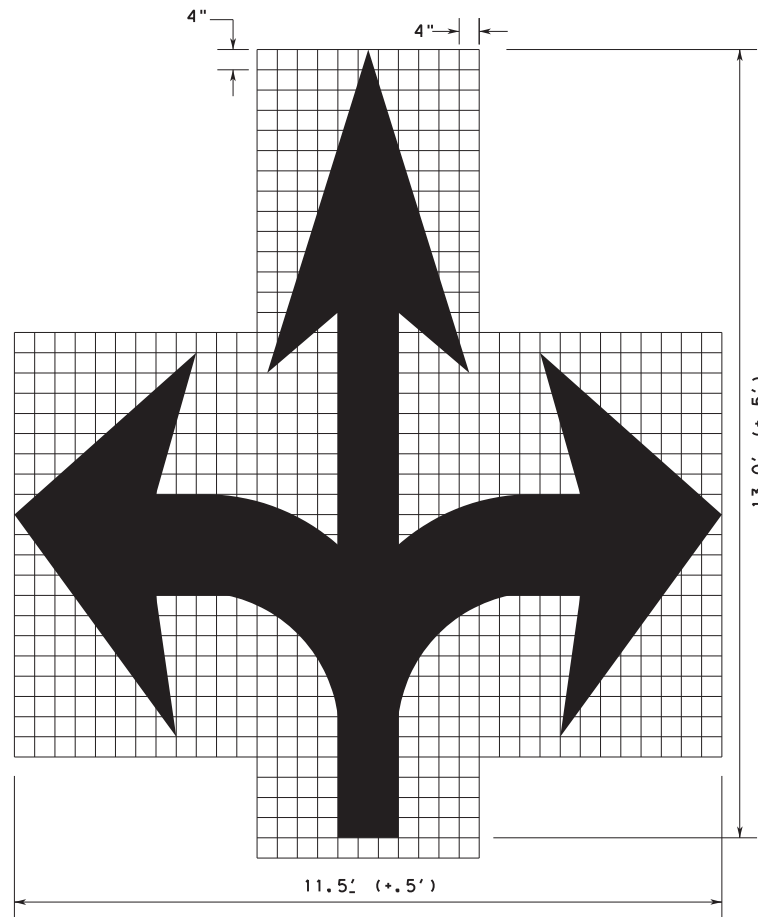
TRAFFIC ENGINEERING STANDARDS

LEFT-TURN "ONLY" AND ARROW
SPACING WORKSHEET

SHEET 1 OF 16

% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: LAN	DSGN. BY: C.B.V.	CHKD. BY: M.E.
SHEET NO.: 16		OF 26





NOTES:

1. MINIMUM 8 FOOT WHITE MARKINGS SHALL BE USED, UNLESS OTHERWISE NOTED. IF MESSAGE CONSISTS OF MORE THAN ONE WORD, IT SHOULD BE PLACED WITH FIRST WORD NEAREST THE DRIVER.
2. THESE DETAILS ARE STANDARD SIZE FOR NORMAL INSTALLATION; SIZES MAY BE REDUCED APPROXIMATELY ONE-THIRD DEPENDING ON CONDITIONS.
3. THE LONGITUDINAL SPACE BETWEEN MARKINGS SHOULD BE 30 FEET.
4. MARKINGS CONSIDERED APPROPRIATE FOR USE WHEN WARRANTED INCLUDE THE FOLLOWING:
 - A. REGULATORY
STOP
RIGHT (LEFT) TURN ONLY
25 MPH
SYMBOL ARROWS
 - B. WARNING
STOP AHEAD
SIGNAL AHEAD
SCHOOL
SCHOOL X-ING
PED X-ING
R X R (SEE RCPM DETAIL)
OTHER WORDS OR SYMBOLS MAY BE NECESSARY UNDER CERTAIN CONDITIONS
5. UNCONTROLLED USE OF PAVEMENT MARKINGS CAN RESULT IN DRIVER CONFUSION. WORD AND SYMBOL MARKINGS SHOULD BE NO MORE THAN THREE LINES.
6. THE WORD "STOP" SHALL NOT BE USED ON THE PAVEMENT UNLESS ACCOMPANIED BY A STOP LINE AND STOP SIGN. THE WORD "STOP" SHALL NOT BE PLACED ON THE PAVEMENT IN ADVANCE TO A STOP LINE, UNLESS EVERY VEHICLE IS REQUIRED TO STOP AT ALL TIMES.
7. PAVEMENT MARKINGS SHOULD GENERALLY BE NO MORE THAN ONE LANE IN WIDTH, WITH SCHOOL MESSAGES BEING THE EXCEPTION. FOR DETAILS OF SCHOOL AND SCHOOL CROSSING PAVEMENT MARKINGS, REFER TO PART VII OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
8. SPACING BETWEEN LETTERS SHOULD BE APPROXIMATELY 4 INCHES. THE WIDTH OF LETTERS MAY VARY DEPENDING ON THE WIDTH OF THE TRAVEL LANES.
9. LANE-USE ARROW MARKINGS MAY BE USED TO CONVEY EITHER GUIDANCE OR MANDATORY MESSAGES. ARROWS USED TO CONVEY A MANDATORY MOVEMENT MUST BE ACCOMPANIED BY STANDARD SIGNS AND THE PAVEMENT MARKING WORD "ONLY".
10. PAVEMENT MARKINGS ARE TO BE LOCATED AS SPECIFIED ELSEWHERE IN THE PLANS.

SEPTEMBER 2009

CITY OF SAN ANTONIO

DEPARTMENT OF PUBLIC WORKS

TRAFFIC ENGINEERING STANDARDS

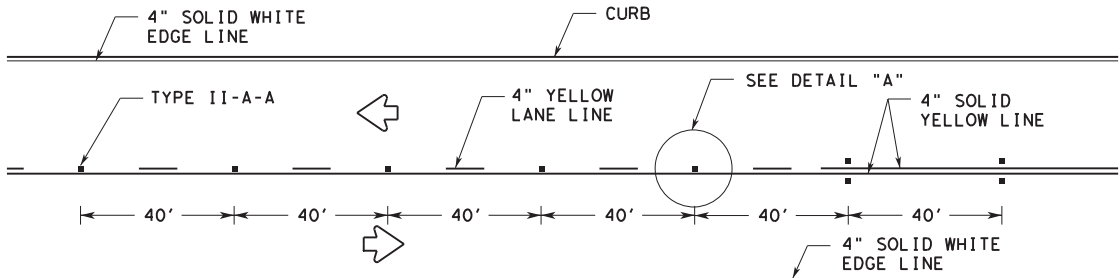
STANDARD PAVEMENT MARKINGS
(ARROWS)

SHEET 3 OF 16

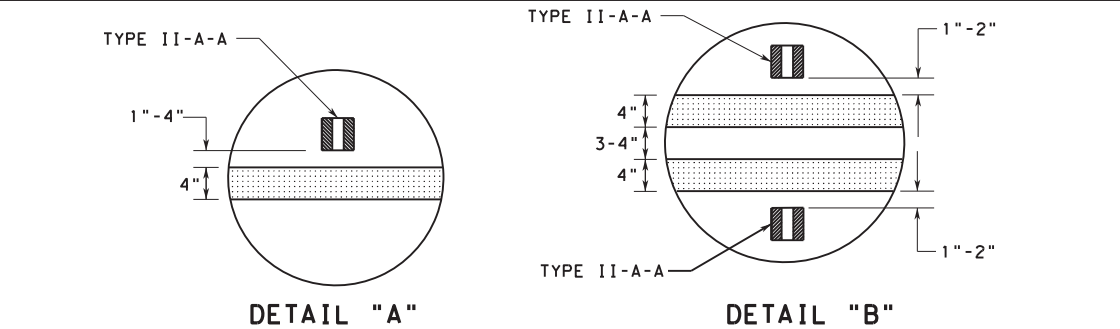
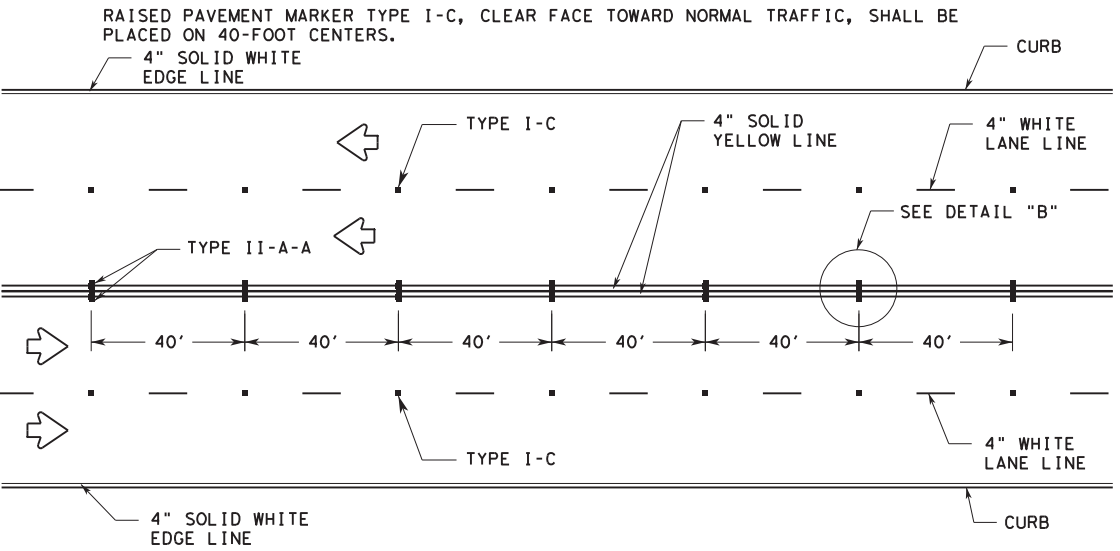
% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: LAN	DSGN. BY: C.B.V.	CHKD. BY: M.E.

SHEET NO.: 18 OF 25

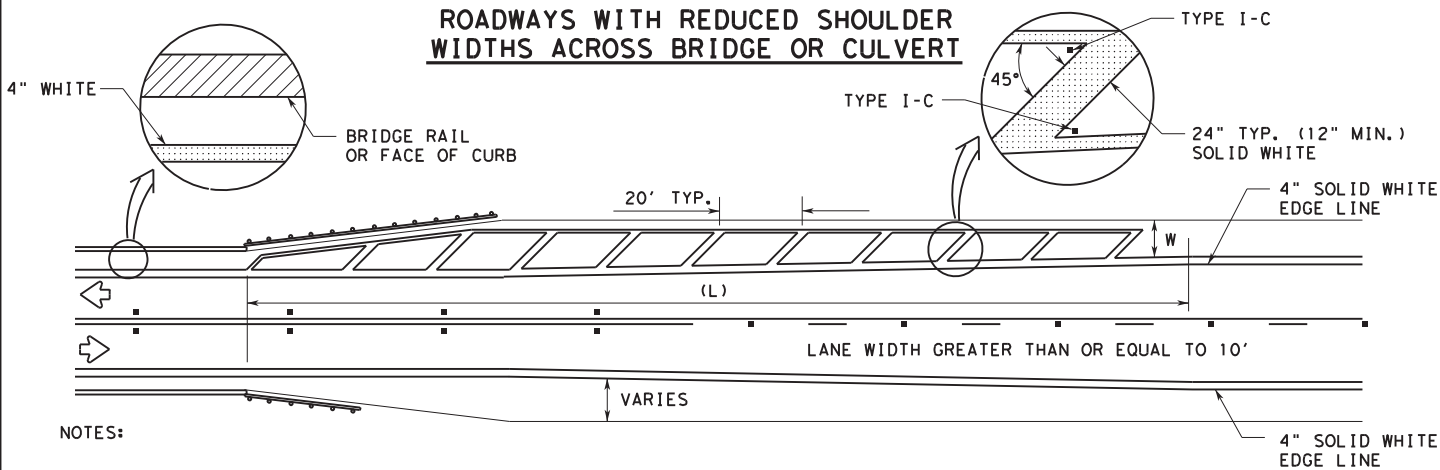
CENTERLINE & EDGE FOR ALL TWO LANE STREETS WITH PASSING ZONE



CENTERLINE, LANE LINES & EDGE LINES FOR FOUR LANE TWO-WAY STREETS



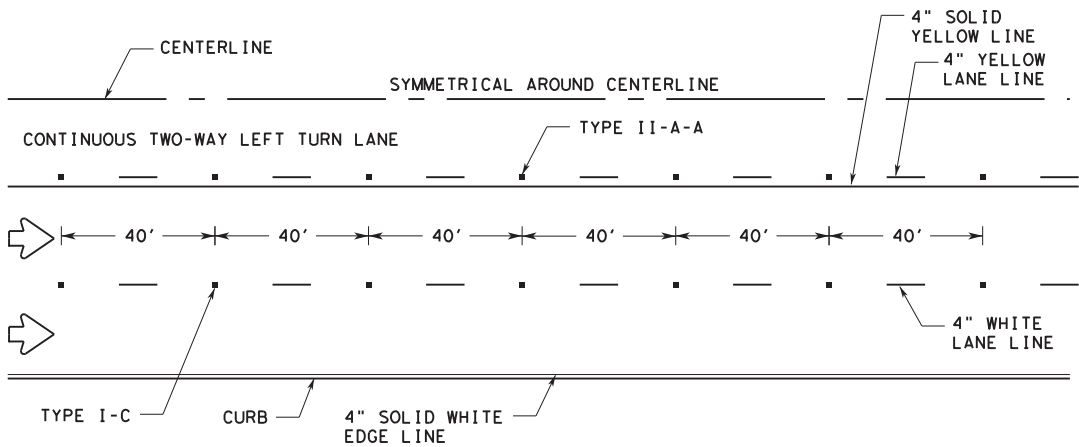
ROADWAYS WITH REDUCED SHOULDER WIDTHS ACROSS BRIDGE OR CULVERT



NOTES:

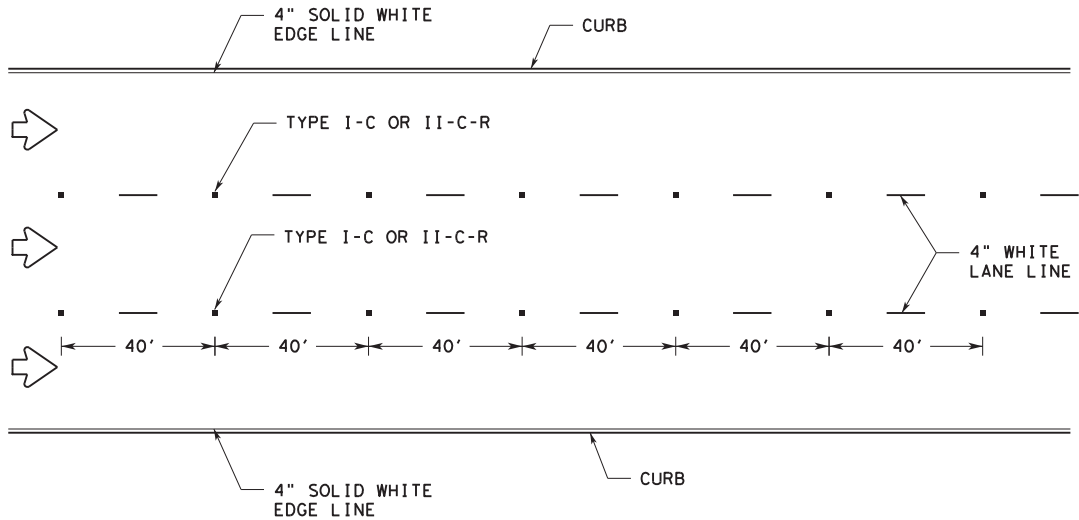
1. NO-PASSING ZONE ON BRIDGE APPROACH IS OPTIONAL BUT IF USED, IT SHALL BE A MINIMUM 500 FEET LONG.
2. FOR CROSSHATCHING LENGTH (L) SEE TABLE 1.
3. THE WIDTH OF THE OFFSET (W) AND THE REQUIRED CROSSHATCHING WIDTH IS THE FULL SHOULDER WIDTH IN ADVANCE OF THE BRIDGE.
4. THE CROSSHATCHING SHOULD BE REQUIRED IF THE SHOULDER WIDTH IN ADVANCE OF THE BRIDGE IS 4 FOOT OR WIDER AND ANY REDUCTION IN SHOULDER WIDTH ACROSS THE BRIDGE OCCURS.

CENTERLINE, LANE LINES, & EDGE LINES FOR TWO-WAY LEFT TURN LANE



LANE LINES & EDGE LINES FOR ONE-WAY MULTILANE STREET

RAISED PAVEMENT MARKERS TYPE II-C-R SHALL HAVE CLEAR FACE TOWARD NORMAL TRAFFIC AND RED FACE TOWARD WRONG-WAY TRAFFIC.



GUIDE FOR PLACEMENT OF STOP LINES, EDGE LINE & CENTERLINE

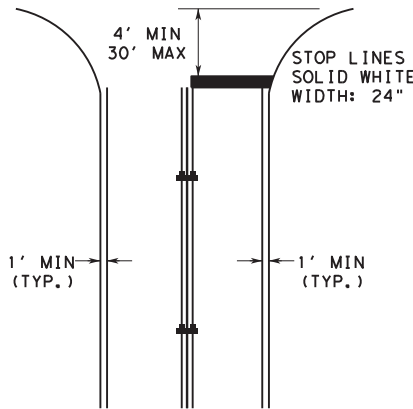


TABLE 1 - TYPICAL LENGTH (L)

* POSTED SPEED	FORMULA
45 >	$L = \frac{WS^2}{60}$
≥ 45	$L = WS$

* 85TH PERCENTILE SPEED MAY BE USED ON ROADS WHERE TRAFFIC SPEEDS NORMALLY EXCEED THE POSTED SPEED LIMIT. CROSSHATCHING LENGTH SHOULD BE ROUNDED UP TO NEAREST 5 FOOT INCREMENT.

L = LENGTH OF CROSSHATCHING (FT)
W = WIDTH OF OFFSET (FT)
S = POSTED SPEED (MPH)

EXAMPLES:
AN 8 FOOT SHOULDER IN ADVANCE OF A BRIDGE REDUCES TO 4 FEET ON A 70 MPH ROADWAY. THE LENGTH OF THE CROSSHATCHING SHOULD BE:
 $L = 8 \times 70 = 560$ FT
A 4 FOOT SHOULDER IN ADVANCE OF A BRIDGE REDUCES TO 2 FEET ON A 40 MPH ROADWAY. THE LENGTH OF THE CROSSHATCHING SHOULD BE:
 $L = 4(40) / 60 = 106.67$ FT ROUNDED TO 110 FT

YIELD LINES



GENERAL NOTES:

1. EDGELINE ADJACENT TO CURB AND GUTTER IS NOT REQUIRED IN ALL CASES, HOWEVER SHALL BE PLACED AS DIRECTED BY CITY TRAFFIC ENGINEER.
2. THE TRAVELED WAY INCLUDES ONLY THAT PORTION OF THE ROADWAY USED FOR VEHICULAR TRAVEL AND NOT THE PARKING LANES, SIDEWALKS, BERMS AND SHOULDERS. THE TRAVELED WAYS SHALL BE MEASURED FROM THE INSIDE OF EDGELINE TO INSIDE OF EDGELINE OF A TWO LANE ROADWAY.
3. ALL RAISED PAVEMENT MARKERS PLACED IN BROKEN LINES SHALL BE PLACED IN LINE WITH AND MIDWAY BETWEEN THE STRIPES.
4. ON CONCRETE PAVEMENTS THE RAISED PAVEMENT MARKERS SHOULD BE PLACED TO ONE SIDE OF THE LONGITUDINAL JOINTS.
5. ALL PAVEMENT MARKING MATERIAL SHALL MEET THE REQUIRED MATERIAL SPECIFICATIONS AS SPECIFIED BY CITY OF SAN ANTONIO STANDARD SPECIFICATIONS.
6. 4" SOLID WHITE EDGE LINES ARE OPTIONAL AS DIRECTED BY THE CITY TRAFFIC ENGINEER.

SEPTEMBER 2009

CITY OF SAN ANTONIO

DEPARTMENT OF PUBLIC WORKS

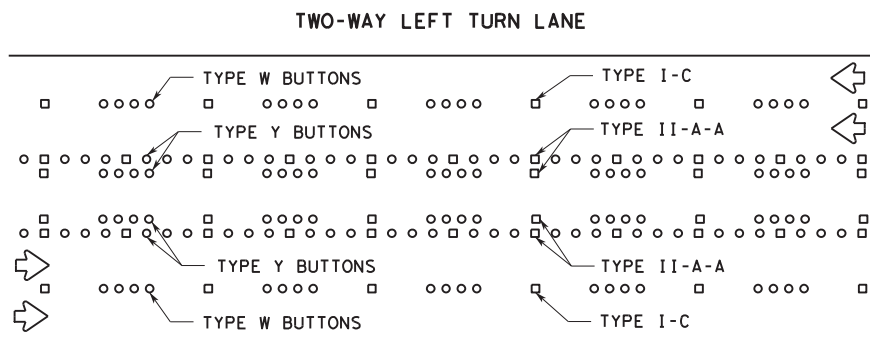
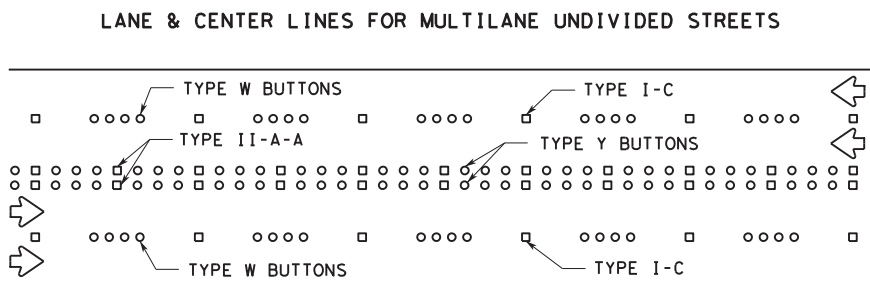
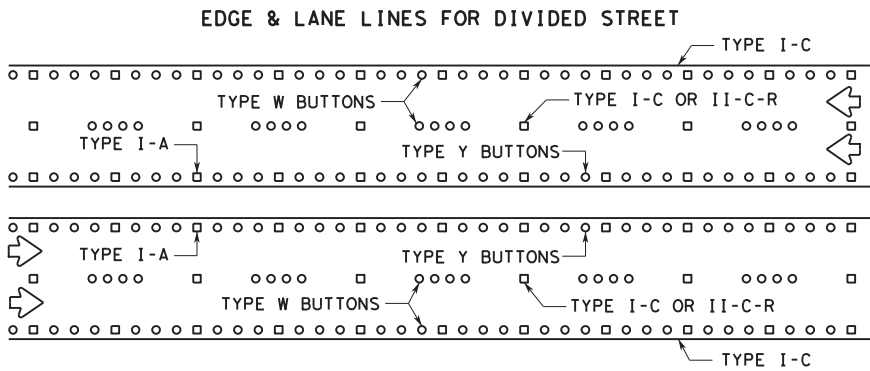
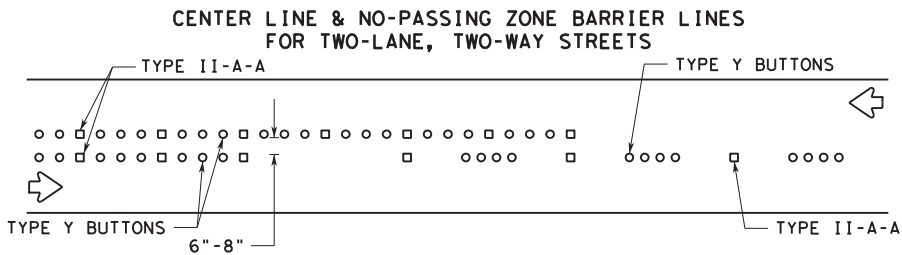
TRAFFIC ENGINEERING STANDARDS

STANDARD PAVEMENT MARKINGS WITH REFLECTIVE RAISED PAVEMENT MARKERS FOR POSITION GUIDANCE 1

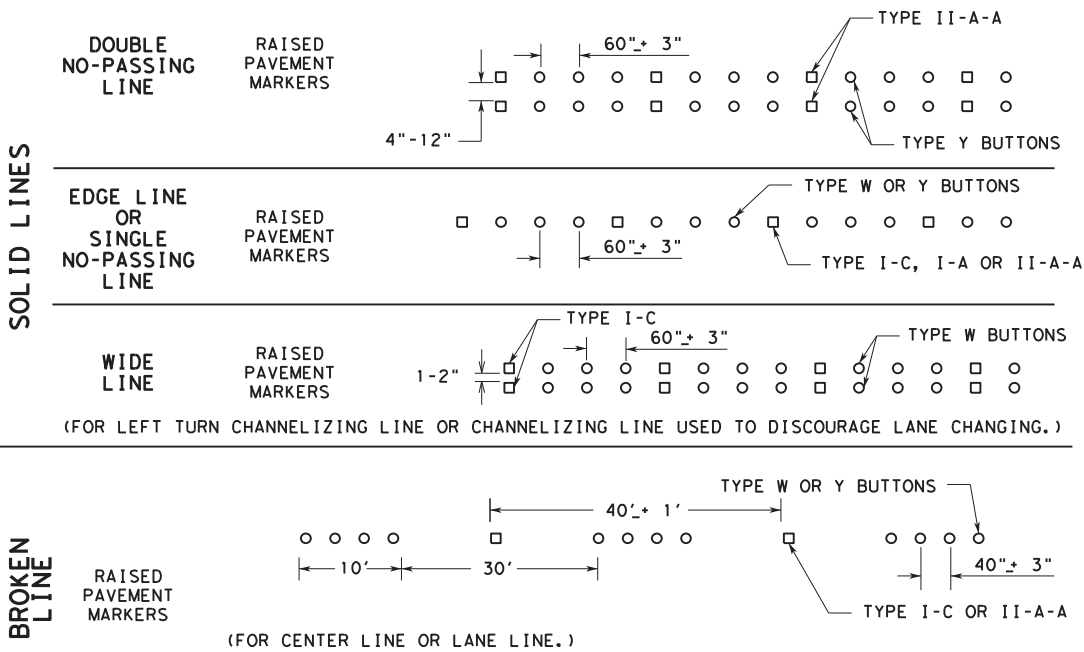
SHEET 4 OF 16

DRWN. BY: LAN DSGN. BY: C.B.V. CHKD. BY: M.E. SHEET NO.: 19 OF 25

RAISED PAVEMENT MARKING PLACEMENT PATTERNS
PLACED W/ REFLECTION PAVEMENT MARKERS (OPTIONAL)

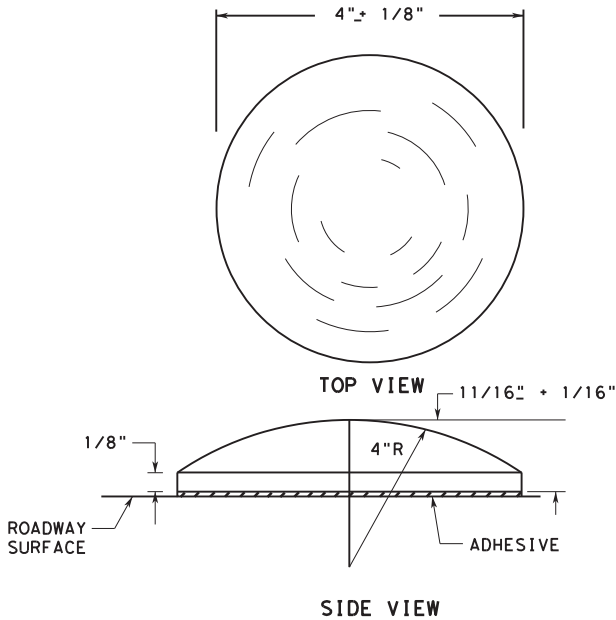


RAISED PAVEMENT MARKINGS PLACEMENT DETAILS
PLACED W/ REFLECTION PAVEMENT MARKERS (OPTIONAL)



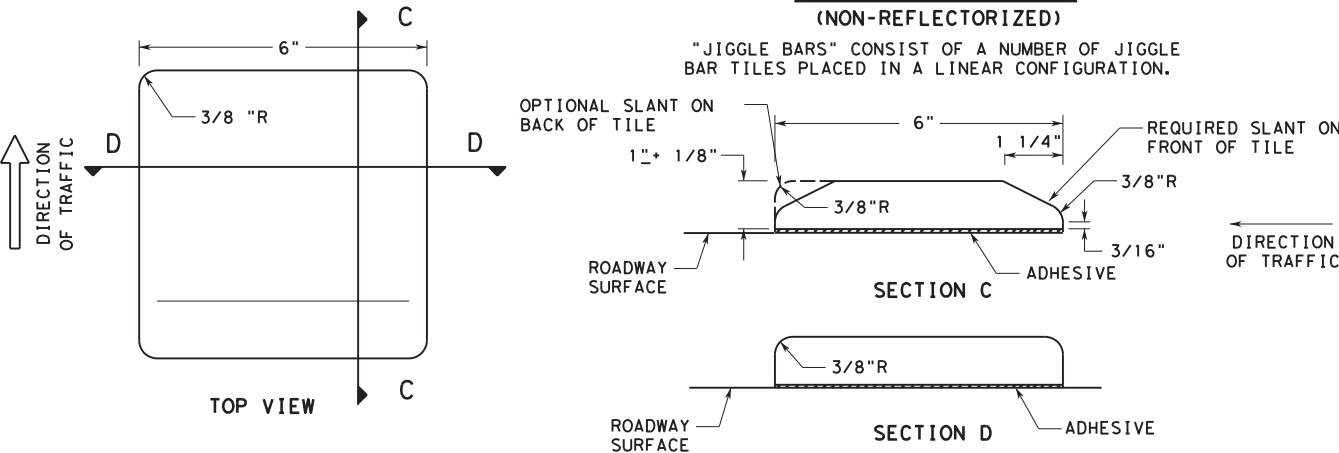
TRAFFIC BUTTONS
(NON-REFLECTORIZED)

NOTE: MINIMUM AREA OF MARKERS SHALL BE NOT LESS THAN 12.5 SQUARE INCHES.



JIGGLE BAR TILES
(NON-REFLECTORIZED)

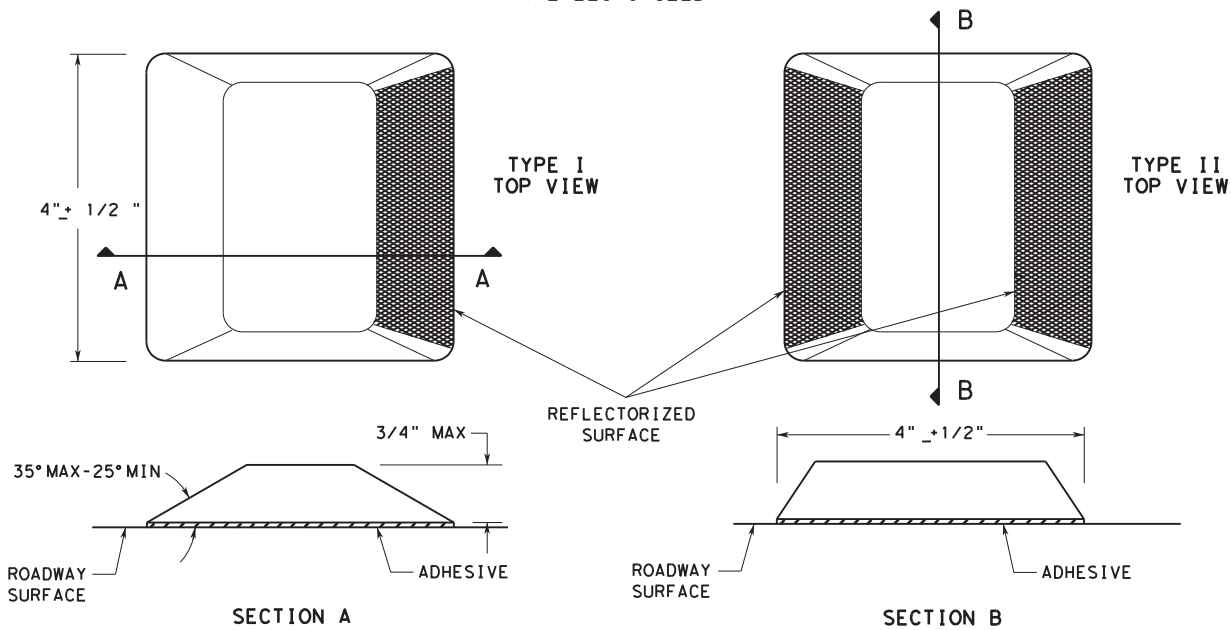
"JIGGLE BARS" CONSIST OF A NUMBER OF JIGGLE BAR TILES PLACED IN A LINEAR CONFIGURATION.



NOTES:

1. RAISED PAVEMENT MARKERS (RPMs) MAY CONSIST OF TRAFFIC BUTTONS, PAVEMENT MARKERS AND/OR JIGGLE BAR TILES. PAVEMENT SURFACE SHALL BE PREPARED AND CLEANED SUBJECT TO APPROVAL OF THE CITY TRAFFIC ENGINEER BEFORE ADHESIVE AND RPMs ARE PLACED.
2. JIGGLE BARS SHALL BE ORIENTED PERPENDICULAR TO ROADWAY. JIGGLE BARS SHALL ALSO BE PLACED AT SUCH OTHER LOCATIONS AS SHOWN IN PLANS OR AS DIRECTED BY THE CITY TRAFFIC ENGINEER.
3. MARKERS, BUTTONS AND JIGGLE BAR TILES SHOWN ARE FOR ILLUSTRATION PURPOSES ONLY AND NOT INTENDED TO SPECIFY ANY PARTICULAR PRODUCT. ALL PAVEMENT MARKERS PROVIDED SHALL BE OF THE SAME MANUFACTURER.
4. ALL DIMENSIONS ARE \pm 1/8" UNLESS OTHERWISE NOTED.
5. ALL PAVEMENT MARKING MATERIALS SHALL MEET MATERIAL SPECIFICATIONS AS SPECIFIED BY CITY OF SAN ANTONIO STANDARD SPECIFICATIONS.
6. TRAFFIC BUTTONS AND JIGGLE BAR TILES ARE TO BE USED ONLY FOR TEMPORARY TRAFFIC CONTROL OR AS DIRECTED BY THE CITY TRAFFIC ENGINEER.

RAISED PAVEMENT MARKERS
(REFLECTORIZED)



SEPTEMBER 2009

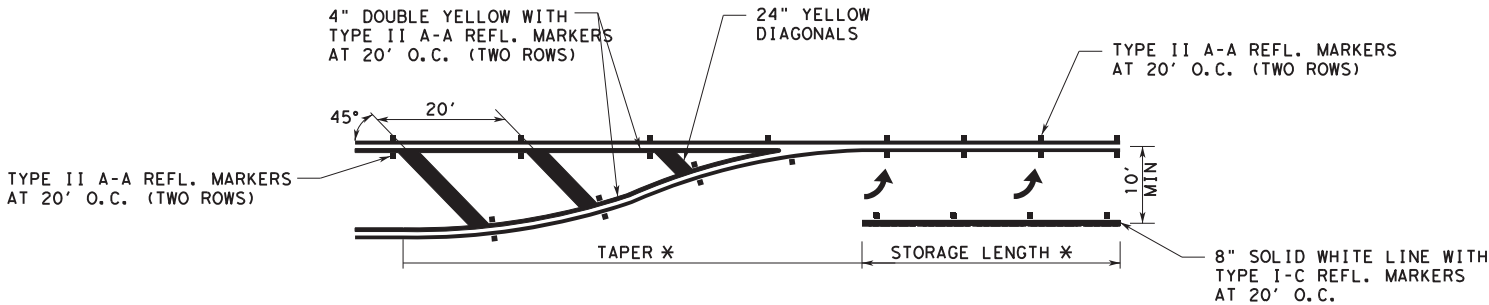
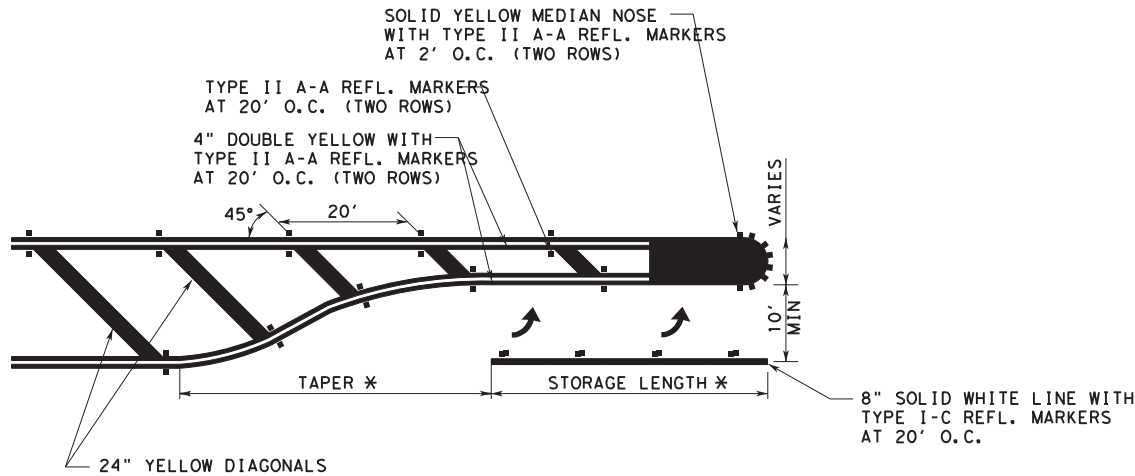
CITY OF SAN ANTONIO
DEPARTMENT OF PUBLIC WORKS

TRAFFIC ENGINEERING STANDARDS
RAISED PAVEMENT MARKERS, REFLECTIVE
PAVEMENT MARKERS, TRAFFIC BUTTONS &
JIGGLE BAR TILES 2
SHEET 5 OF 16

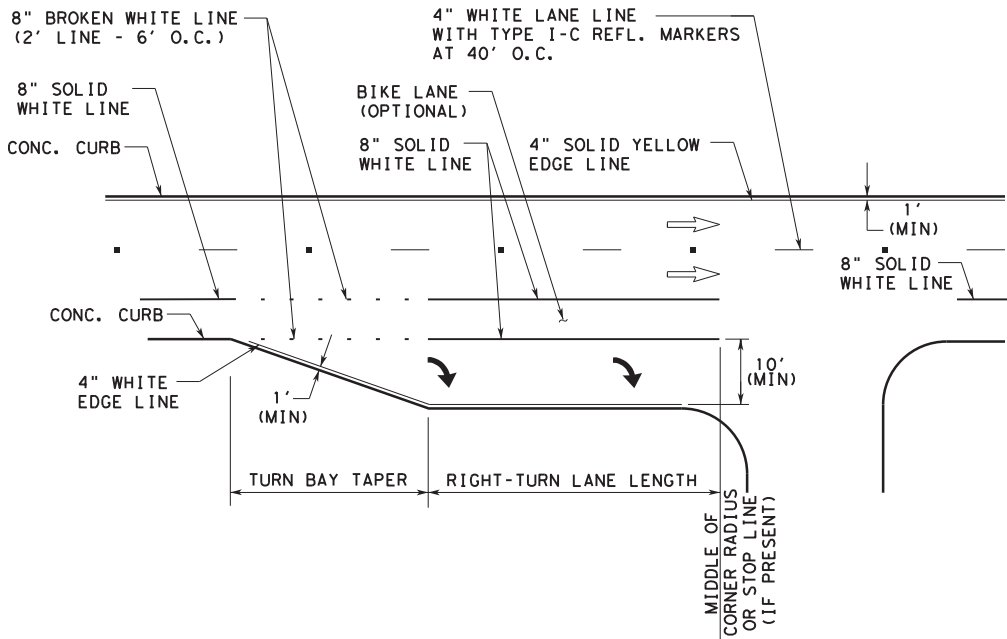
% SUBMITTAL	PROJECT NO.:	DATE:
DRWN. BY: LAN	DSGN. BY: C.B.V.	CHKD. BY: M.E.
		SHEET NO.: 20 OF 25

PAINTED MEDIAN LEFT TURN BAY DETAILS

* - USE MINIMUM TURN BAY REVERSE CURVE TAPER LENGTH AND MINIMUM STORAGE LENGTH TABLES FOR "LEFT-TURN LANE (RAISED MEDIAN)" ON SHEET 10 OF 16.



UNSIGNALIZED RIGHT-TURN LANE



MINIMUM TURN BAY TAPER LENGTH	
POSTED SPEED (MPH)	LENGTH (FT)
30 OR LESS	90'
35 OR MORE	120'

MINIMUM RIGHT-TURN LANE LENGTH	
POSTED SPEED (MPH)	LENGTH (FT)
40 OR LESS	110'
45 OR MORE	150'

NOTES:

1. THE POSTED SPEED LIMIT IS TYPICALLY EQUAL TO THE DESIGN SPEED MINUS 5 MPH.
2. THE DIMENSIONS GIVEN FOR DUAL LEFT (RAISED MEDIAN) IN THE MINIMUM LENGTH TABLES ON THIS SHEET ARE ALSO APPLICABLE FOR DUAL RIGHT-TURN LANES.
3. STORAGE LENGTHS LONGER THAN THE MINIMUMS LISTED ON THIS DRAWING MAY BE DETERMINED USING TRAFFIC ENGINEERING ANALYSIS OR APPROXIMATE CALCULATIONS.
4. FOR THE PLACEMENT OF PAVEMENT ARROWS AND WORDS SEE LEFT-TURN "ONLY" AND ARROW SPACING WORKSHEET.
5. REFER TO APPLICABLE STANDARD PAVEMENT MARKINGS WITH REFLECTIVE RAISED PAVEMENT MARKERS FOR POSITION GUIDANCE AND LEFT-TURN & RIGHT-TURN LANE STANDARD PAVEMENT MARKINGS WITH REFLECTIVE RAISED PAVEMENT MARKINGS.
6. REFER TO BICYCLE LANE PAVEMENT MARKINGS STANDARD FOR TYPE AND PLACEMENT.
7. 4" SOLID WHITE AND YELLOW EDGE LINES ARE OPTIONAL AS DIRECTED BY THE CITY TRAFFIC ENGINEER.

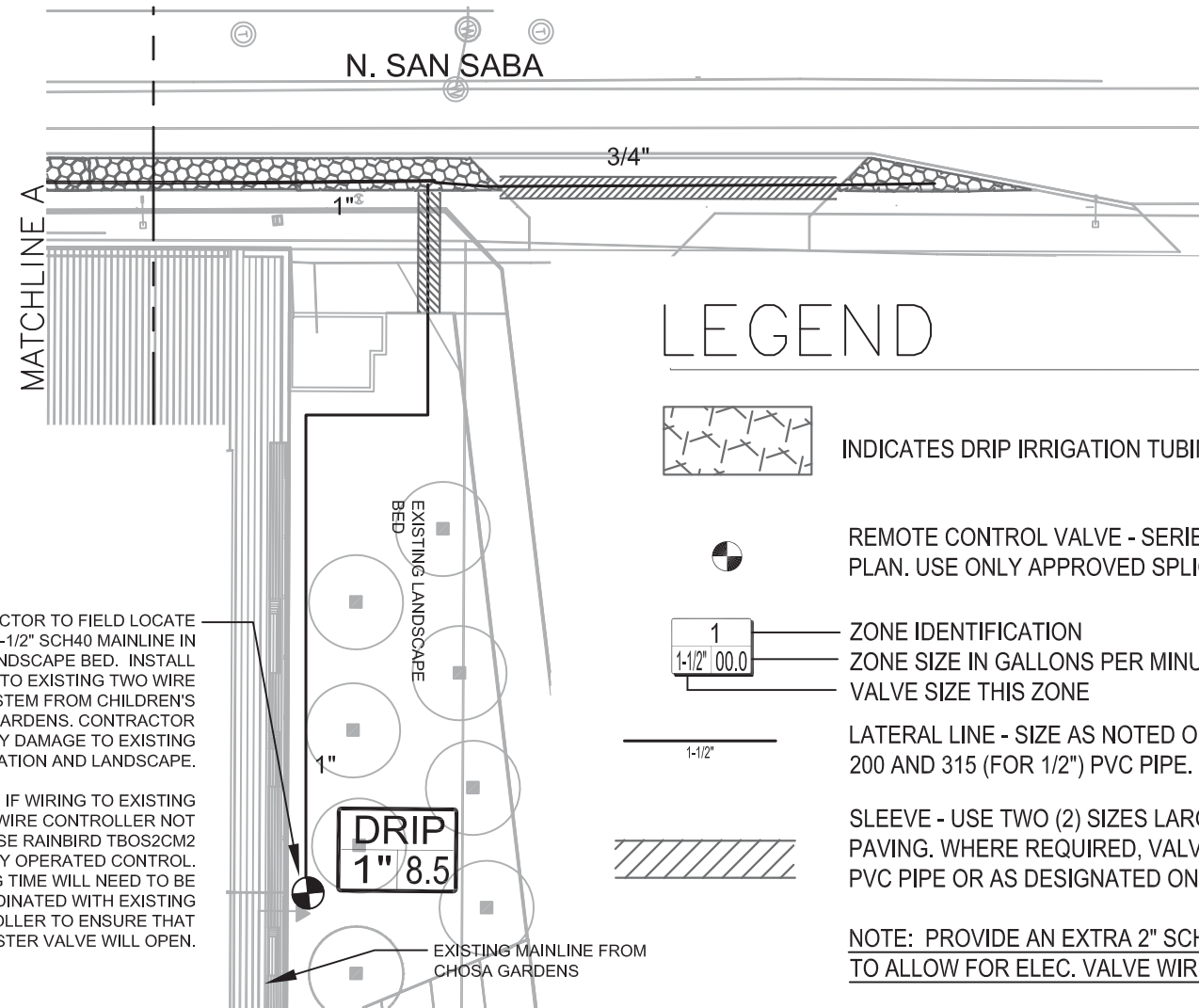
SEPTEMBER 2009

CITY OF SAN ANTONIO
DEPARTMENT OF PUBLIC WORKS

TRAFFIC ENGINEERING STANDARDS
LEFT-TURN LANE & RIGHT-TURN LANE
DESIGN WORKSHEET 2
SHEET 11 OF 16

% SUBMITTAL PROJECT NO.: DATE:
DRWN. BY: LAN DSGN. BY: C.B.V. CHKD. BY: M.E. SHEET NO.: 21 OF 26

Design Filename: \$FILE\$



1"=30'-0"

1"=30'-0"

ALTERNATE: IF WIRING TO EXISTING
TWO WIRE CONTROLLER NOT
AVAILABLE, USE RAINBIRD TBOS2CM2
BATTERY OPERATED CONTROL.
WATERING TIME WILL NEED TO BE
COORDINATED WITH EXISTING
CONTROLLER TO ENSURE THAT
EXISTING MASTER VALVE WILL OPEN.

EXISTING LANDSCAPE
BED

DRIP	
1"	8.5

- EXISTING MAINLINE FROM
CHOSA GARDENS

NOTE: PROVIDE AN EXTRA 2" SCH 40 PVC SLEEVE NEXT TO ALL MAINLINE SLEEVE LOCATIONS TO ALLOW FOR ELEC. VALVE WIRING FROM CONTROLLER.

This plan is complete and conforms to the design and installation parameters of the irrigation design and equipment standards set out in 35-510(j) and 35-511(c)(6) of the City of San Antonio Unified Development Code.



7-2



NO	DATE	DESCRIPTION	DWG	CHK	
		REVISIONS			

POZNECKI
INC
CAMARILLO

(210) 349-3273
(210) 349-4395 (FAX)

5835 CALLAGHAN RD. SUITE 200
SAN ANTONIO, TEXAS, 78228

http://www.pozcam.com/

BP REG. NO. F-483



DGN:				
CHK DGN:	SG			
DWG:		STATE	COUNTY	SHEET NO.
CHK DWG:	SG	TEXAS	BEXAR	OF

\$TIMES\$

Plotted on: \$DATE\$

Design Filename: \$FILE\$

GENERAL NOTES

1. SPRINKLER LINES AS SHOWN ARE DIAGRAMMATIC BUT SHOULD BE REASONABLY FOLLOWED. LOCATIONS OF SPRINKLER HEADS SHALL BE ESTABLISHED BY THE CONTRACTOR BUT DESIGN SPACING MAY NOT BE EXCEEDED WITHOUT AUTHORIZATION FROM LANDSCAPE ARCH. IRRIGATION TRENCHING SHALL ADHERE TO TREE PROTECTION PLANS & DETAILS. REFER TO CIVIL DRAWINGS SHEETS 4 & 6 AND L0.02. NO TRENCHING SHALL OCCUR WITHIN 5' RADIUS OF TREE TRUNK UNLESS OTHERWISE APPROVED BY THE LANDSCAPE ARCHITECT.
2. INSTALL ALL VALVES IN 12" RECTANGULAR VALVE BOXES. VALVE SHOULD BE CENTERED IN BOX TO ALLOW EASY ACCESS TO SOLENOID ASSEMBLY AND COVER BOLTS.
3. ZONE PIPE SIZING IS PARTICULARLY IMPORTANT ON PROJECTS WHERE LONG RUNS ARE NECESSARY. "RULE OF THUMB" SIZING IS NOT ALLOWED ON THIS PROJECT. SIZING AS SHOWN MUST BE FOLLOWED.
4. INSTALL RAIN & FREEZE SWITCHES IN AN ELEVATED LOCATION CLEAR OF TREES OR OTHER OBSTRUCTIONS. INSTALL IN LOCATION EXPOSED TO EXTERIOR AMBIENT CONDITION.
5. ALLOW A MINIMUM OF 4" CLEARANCE FROM ANY STRUCTURE WHEN INSTALLING SPRINKLER HEADS. THIS INCLUDES SIDEWALKS, CURBS, BUILDINGS, ETC..
6. INSTALL SLEEVING SUCH THAT A MINIMUM OF 6" EXTENDS BEYOND CURBS, WALKS, OR WALLS.
7. AFTER INSTALLATION SYSTEM MUST BE BALANCED BY ADJUSTING FLOW CONTROLS ON VALVES. SPRAY ZONES SHALL BE ADJUSTED TO 25 PSI-30 PSI AT THE FURTHEST HEAD IN EACH ZONE.
8. SYSTEM SHALL REQUIRE MINIMUM OF 62 LBS. STATIC PRESSURE FOR SYSTEM TO OPERATE PROPERLY. IRRIGATION CONTRACTOR SHALL NOTIFY SPRINKLER SYSTEM DESIGNER OF PRESSURE DEFICIENCIES OR ANY OTHER SITE PROBLEMS THAT MAY ALTER THE EFFECTIVENESS OF THE SYSTEM.
9. CONTRACTOR TO PROVIDE TEMPORARY DRIP IRRIGATION FOR EXISTING TREES WITHIN THE LIMITS OF CONSTRUCTION. REFERENCE SHEET L0.02 FOR CONSTRUCTION DETAIL.

Scheduling Controller

Typical Weekly, Based on Precipitation Rate

Total Hours of Operation 2.38

ZONE TYPE	Precipitation Rate (in/hr)	Water Desired Inch/Week	Time per cycle (min)	No. of Zones	Total Minutes	Total Hours
DRIP	0.42	1.00	142.86	1	142.9	2.38

NOTE:

IT WILL BE NECESSARY TO DIVIDE TIME INTO 2 TO 3 CYCLES TO MINIMIZE RUNOFF. A TYPICAL SCHEDULE WOULD ALLOW WATERING TO OCCUR TWO DAYS PER WEEK. TOTAL WATERING TIME WOULD BE DIVIDED BY THE NUMBER OF WATERING DAYS. THIS SCHEDULE IS DESIGNED FOR SUMMER WATER USAGE. CONTRACTOR RESPONSIBLE FOR WATERING SCHEDULE DURING ESTABLISHMENT. FOLLOW LOCAL ORDINANCES DURING DROUGHT RESTRICTIONS

MOST CRITICAL ZONE CHART PRESSURE REQUIREMENT CALCULATIONS AT ZONE DESIGN STATISTICS FOR CALCULATIONS

SPRINKLER HEAD REQUIREMENT:	30 PSI
TOTAL ZONE FLOW:	4.4 GPM
ELECTRIC VALVE SIZE:	
STATIC PRESSURE LESS 10%:	60 PSI
ACCUMULATIVE LOSSES FROM WATER SOURCE TO FURTHEST HEAD	
ZONE PIPE/FITTING LOSS:	1 PSI
1" ELECTRIC VALVE LOSS:	2.3 PSI
ELEVATION NET GAIN/LOSS:	0 PSI
EX.SYSTEM MAINLINE LOSS:	3 PSI
EX.SYSTEM MAIN LOOP LOSS:	1.4 PSI
EX.BACKFLOW PREVENTER LOSS:	5 PSI
EX.WATER METER LOSS:	3.3 PSI
EX. MASTER VALVE LOSS:	2.3 PSI
EX.TYPE K COPPER SERVICE LOSS:	1.3 PSI
TOTAL NET LOSS:	19.6 PSI
DESIGN PRESSURE:	49.6 PSI

NOTE:

IF DESIGN PRESSURE IS LESS THAN STATIC PRESSURE, CONTACT IRRIGATION CONSULTANT.



GARZA
CONSULTING
IRRIGATION DESIGN
PLANNING & COST ANALYSIS
P.O. BOX 681974
SAN ANTONIO, TEXAS
PHONE (210) 490-1191

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CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

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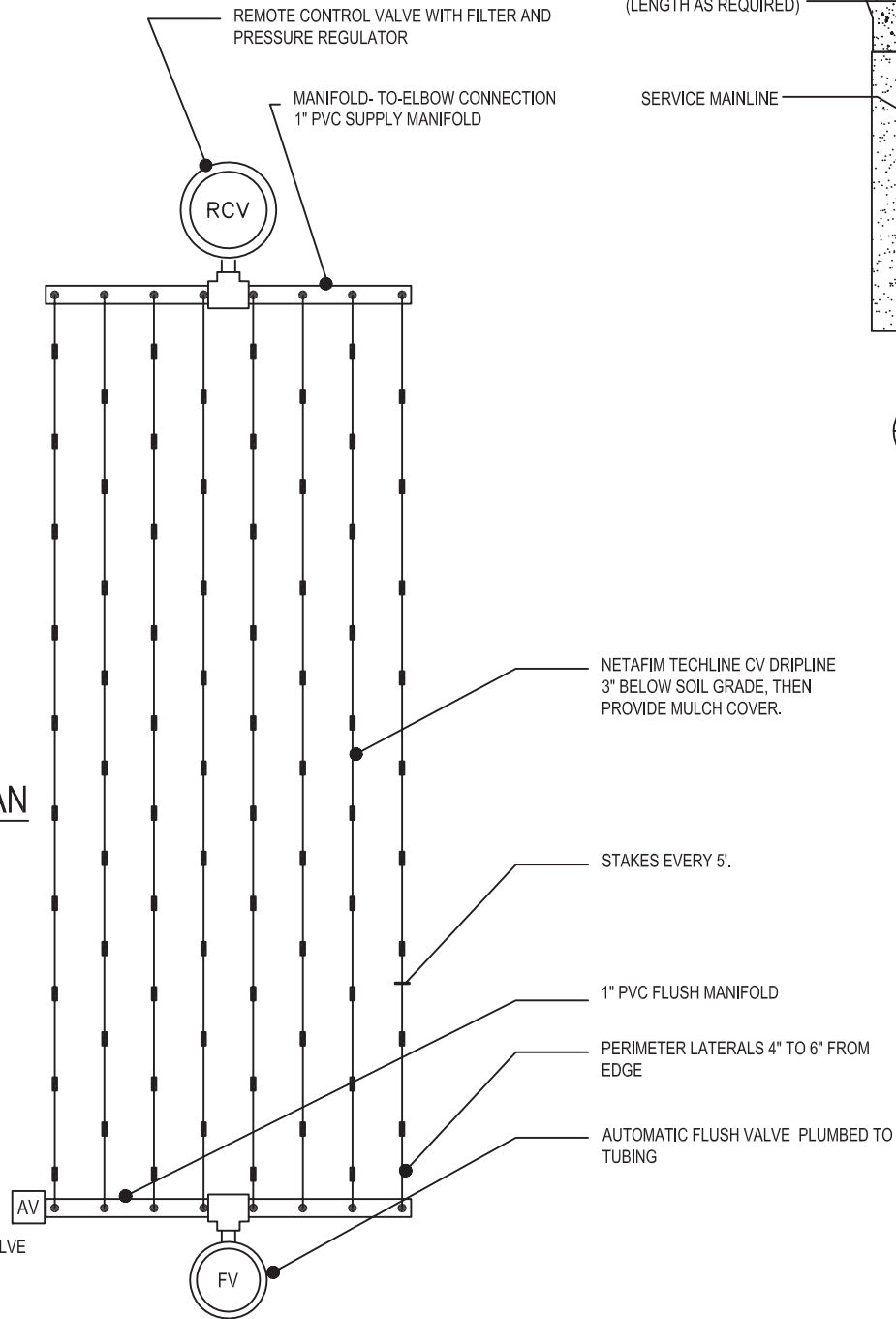
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SIDEWALK IMPROVEMENTS
FROM HOUSTON TO W. MARTIN

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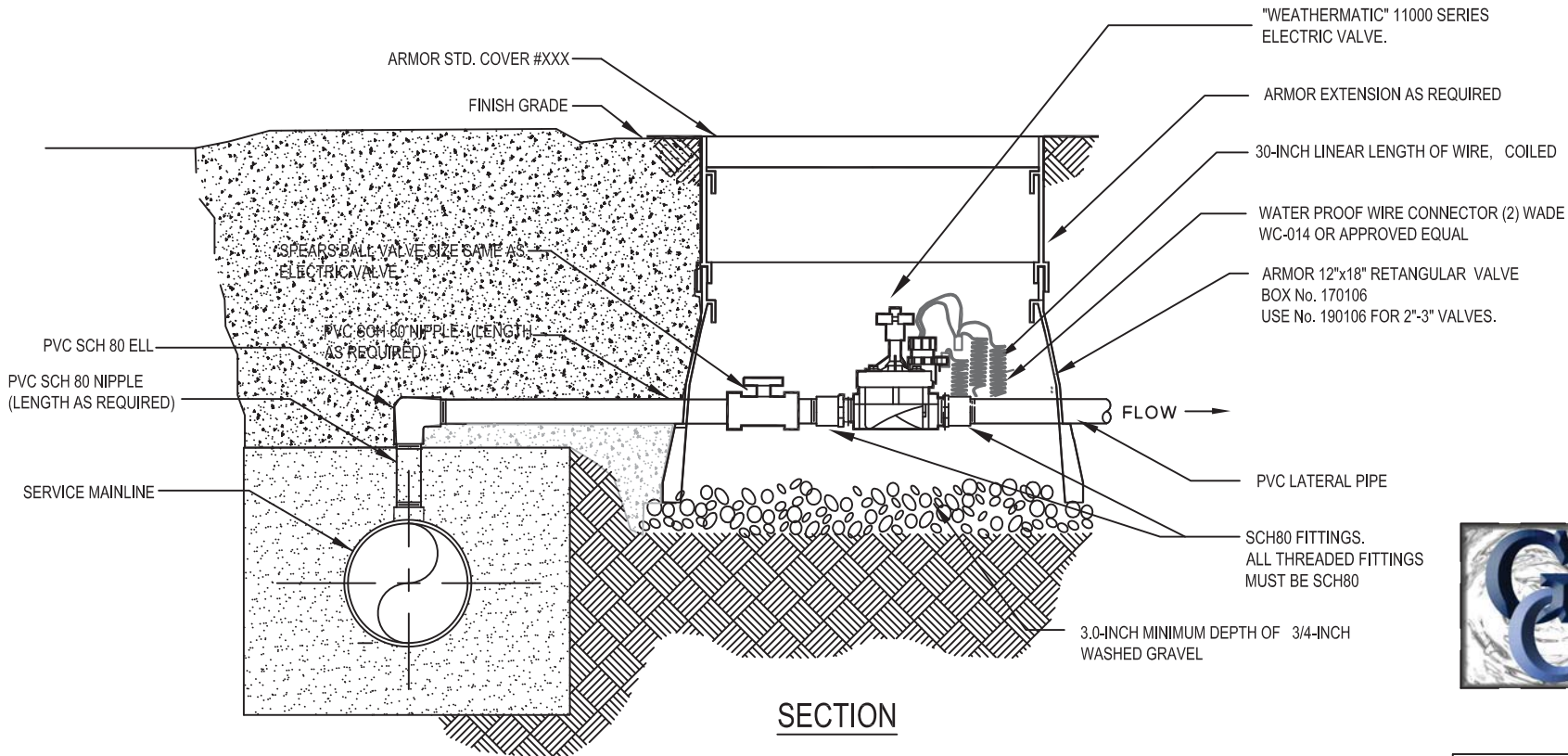
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Plotted on: \$TIMES\$ \$DATE\$ \$FILE\$ Design Filename: \$FILE\$

PLAN

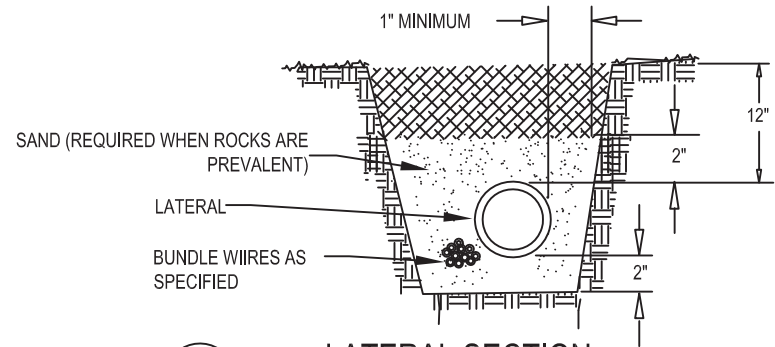


3 DRIP ZONE INSTALLATION DETAIL (TYP.)



1 ELECTRIC VALVE DETAIL AT SERVICE LINE

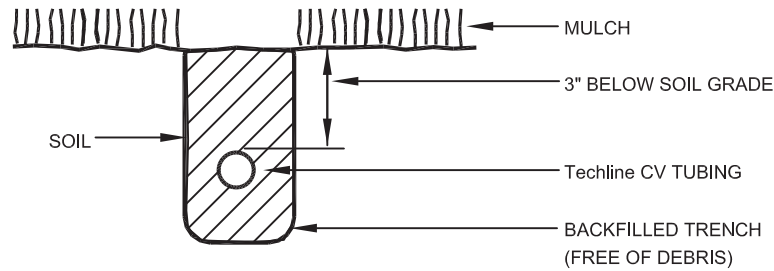
NOT TO SCALE



2 LATERAL SECTION

NOT TO SCALE

SECTION



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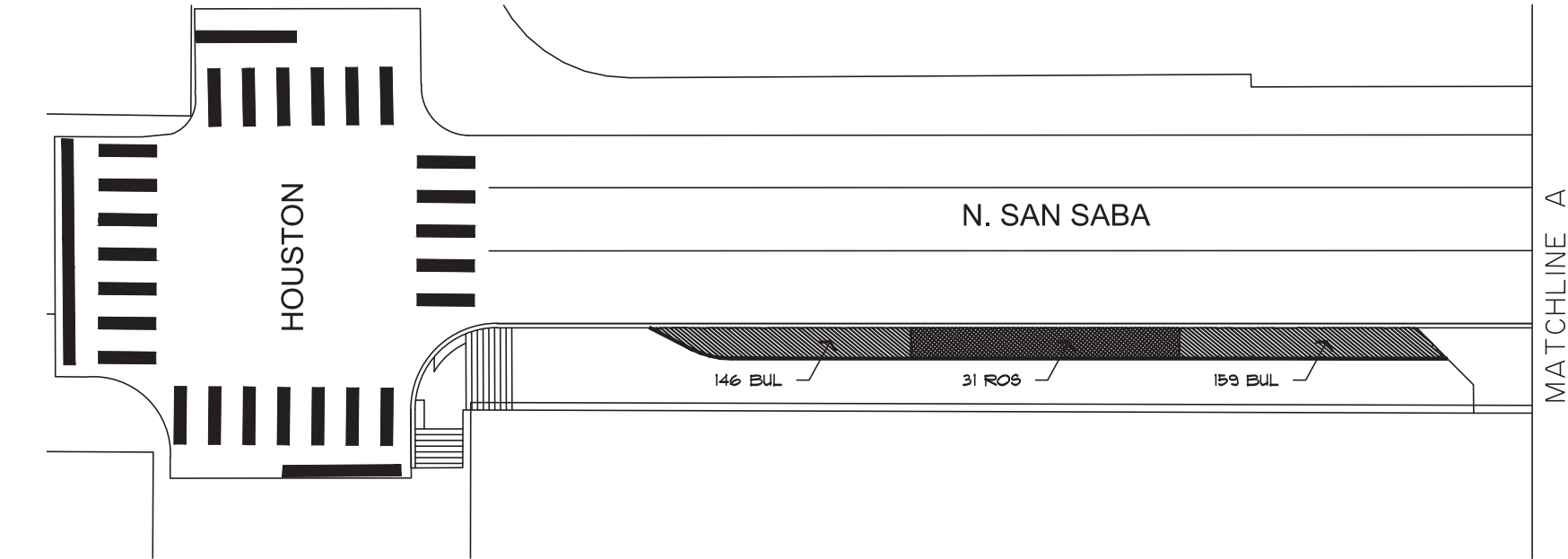
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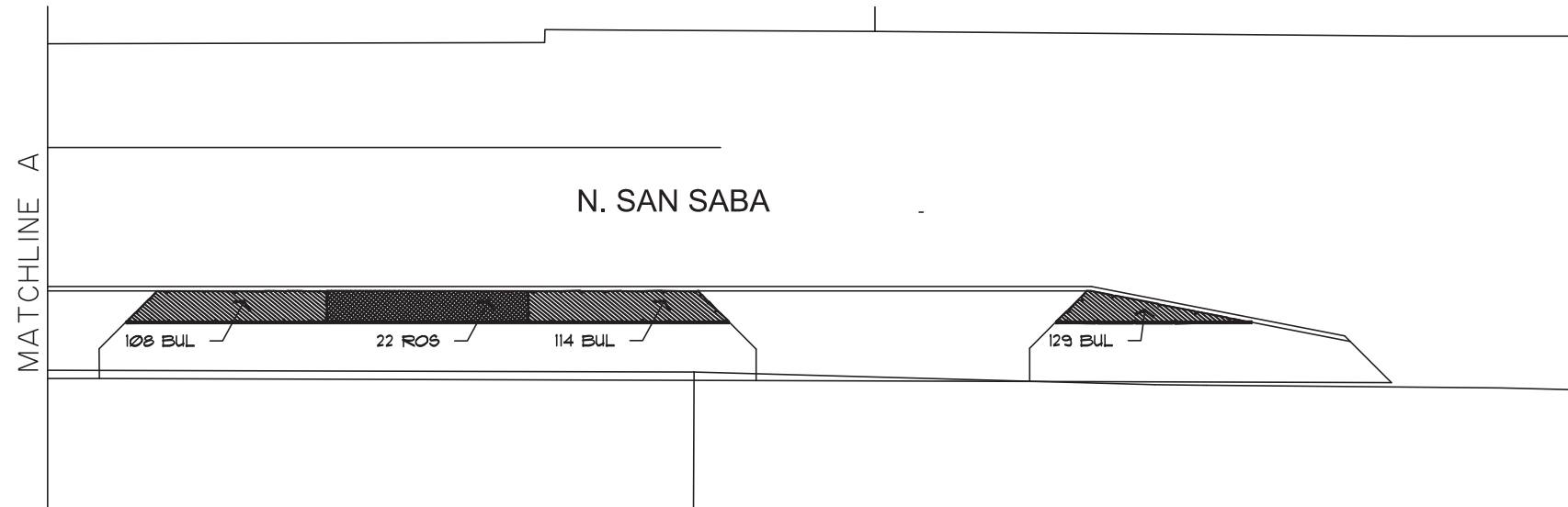
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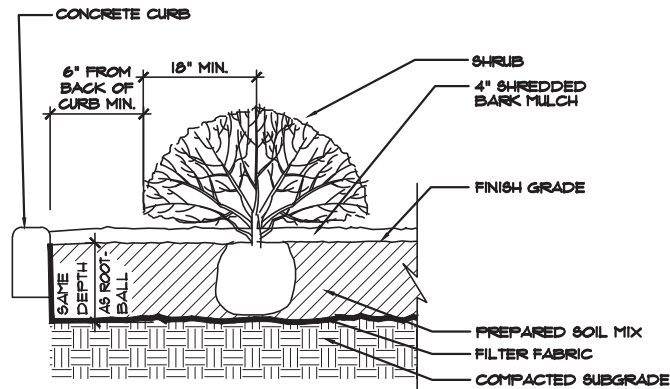
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1 LANDSCAPE PLAN
1"=30'-0"



2 LANDSCAPE PLAN
1"=30'-0"



3 SHRUB BED DETAIL
NTS

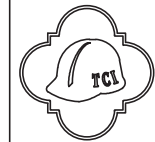
PLANTING NOTES:

- Contractor shall be responsible for making itself familiar with all underground utilities, pipes and structures.
- Do not willingly proceed with construction as designed when it is obvious that unknown obstruction and/or grade differences exist that may not have been known during design. Such conditions shall be immediately brought to the attention of the Landscape Architect. The Contractor shall assume full responsibility for all necessary revisions due to failure to give such notification.
- The Contractor shall be responsible for any coordination with subcontractors as required to accomplish planting operations.
- If conflicts arise between size of areas and plans, Contractor is to contact Landscape Architect for resolution. Failure to make such conflicts known, will result in Contractor's liability to relocate the materials at contractor's expenses.
- Prepared Soil Mix shall be New Earth 4-way landscape mix, available from New Earth Materials, 210-661-5180. Submit product information and 1 gallon of soil mix for approval.
- Mulch shall be shredded hard wood mulch available from New Earth. 210-661-5180. Submit product information and 1 gallon of soil mix for approval.
- Filter Fabric shall be Dewitt's Pro-5 Weed Barrier, 5 oz. woven, needle punched, polypropylene fabric. Submit product sample for approval.

PLAN LIST

KEY	SCIENTIFIC NAME COMMON NAME	CONDITION	REMARKS
SHRUBS			
ROS	Rosmarinus officinalis 'Lockwood de Forest' Rosemary	1 gal.	12" ht., 12" sprd, full, 36" o.c.
BUL	Bulbine frutescens 'Tiny Tangerine' Yellow Bulbine	1 gal.	12" ht., 12" sprd, full, 15 a.c.

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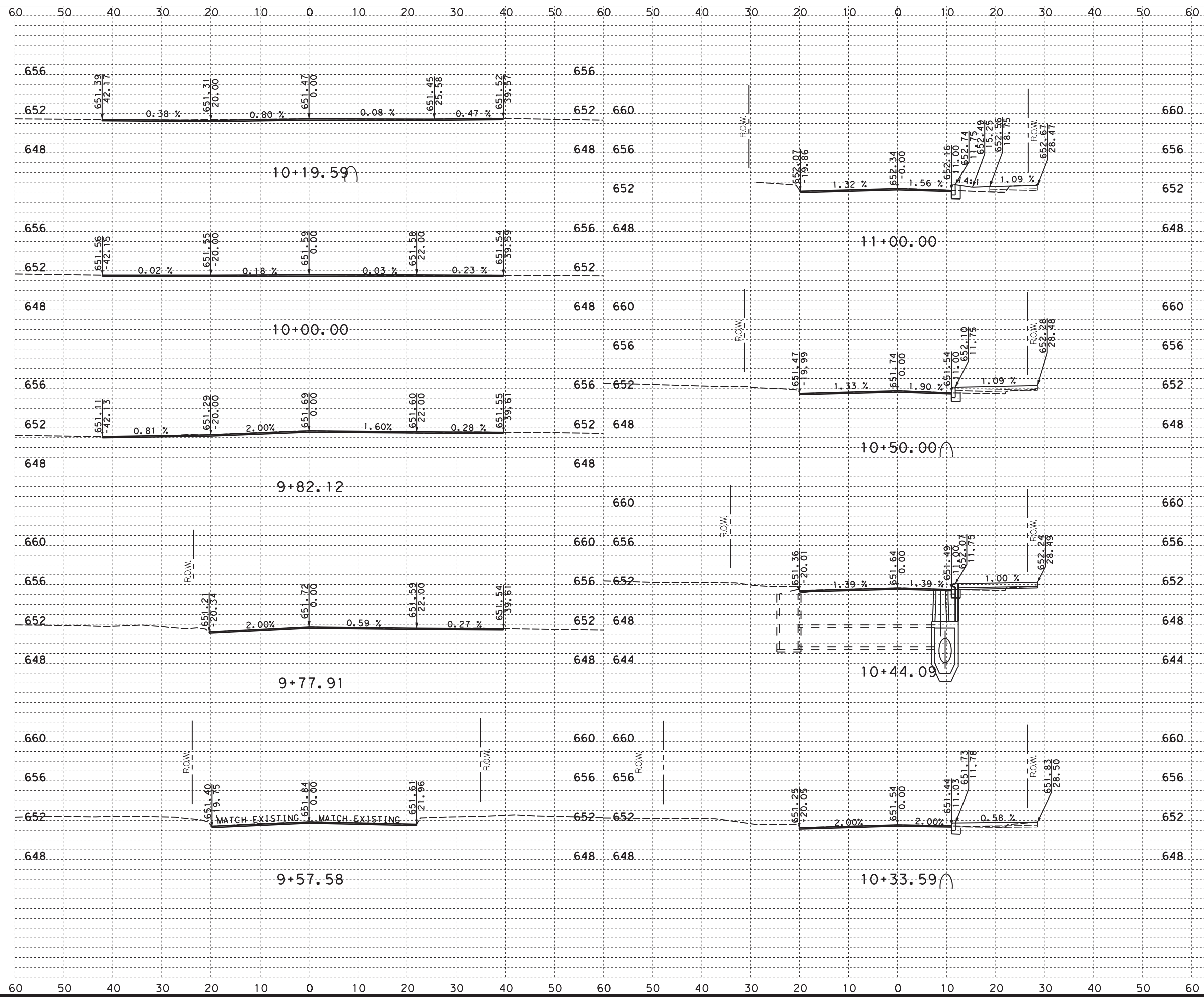
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C.F.Z. Group LLC
Landscape Architecture
& Planning
7410 John Smith Drive, Suite 208
San Antonio, Texas 78229
210-366-1911/210-366-0044 fax



7/ 20/15

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FOR CONTRACTOR
INFORMATION ONLY

SCALE: 1"=20' HORIZ 1"=10' VERT

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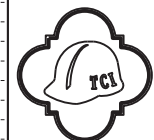


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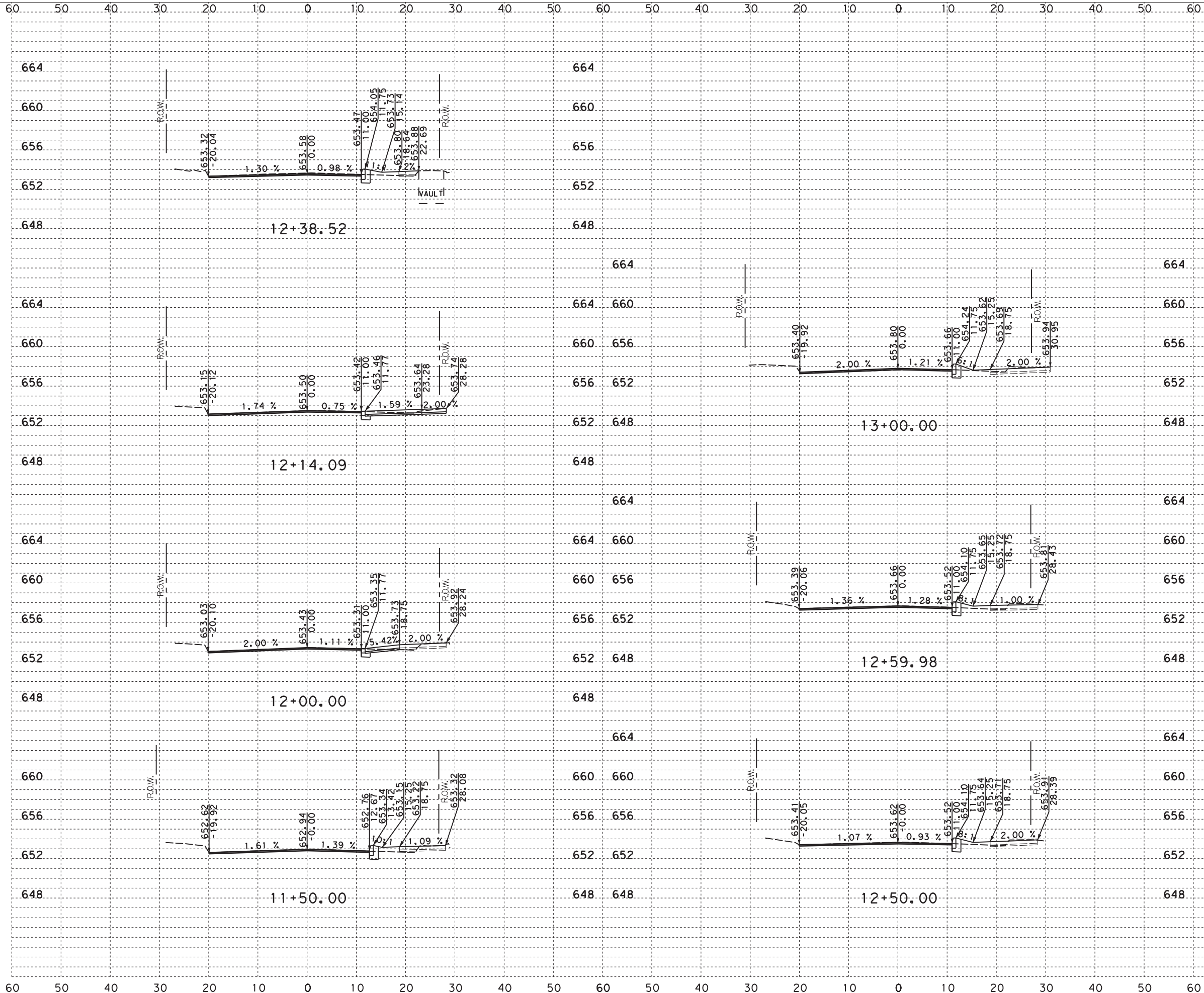
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N SAN SABA SIDEWALK IMPROVEMENTS FROM W HOUSTON TO W MARTIN DESIGN CROSS SECTIONS

1

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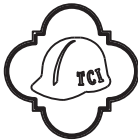
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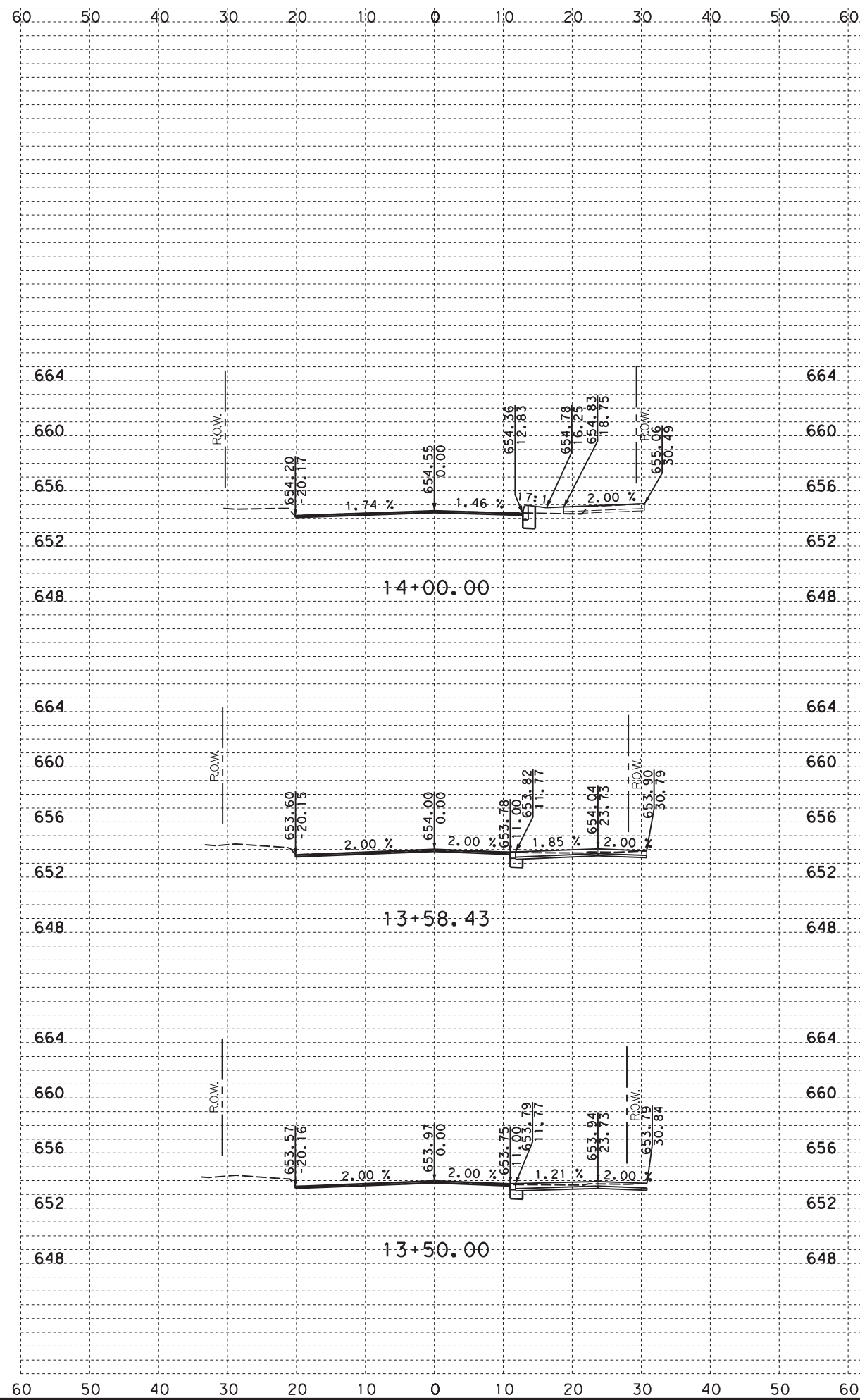
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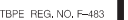
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