

## HISTORIC AND DESIGN REVIEW COMMISSION

April 6, 2016

Agenda Item No: 22

**HDRC CASE NO:** 2016-095  
**ADDRESS:** 415 MARY LOUISE  
**LEGAL DESCRIPTION:** NCB 6703 BLK 13 LOT 3  
**ZONING:** R6 H  
**CITY COUNCIL DIST.:** 7  
**DISTRICT:** Monticello Park Historic District  
**APPLICANT:** David Whitworth  
**OWNER:** David Whitworth  
**TYPE OF WORK:** Carport  
**REQUEST:**

The applicant is requesting a Certificate of Appropriateness for approval to increase the footprint and height of an existing non-historic carport. The new carport will be 22'x25' with a 15' ridge height.

### APPLICABLE CITATIONS:

*Historic Design Guidelines, Chapter 2, Guidelines for Exterior Maintenance and Alterations*

#### 9. Outbuildings, Including Garages

##### A. MAINTENANCE (PRESERVATION)

- i. *Existing outbuildings*—Preserve existing historic outbuildings where they remain.
- ii. *Materials*—Repair outbuildings and their distinctive features in-kind. When new materials are needed, they should match existing materials in color, durability, and texture. Refer to maintenance and alteration of applicable materials above, for additional guidelines.

##### B. ALTERATIONS (REHABILITATION, RESTORATION, AND RECONSTRUCTION)

- i. *Garage doors*—Ensure that replacement garage doors are compatible with those found on historic garages in the district (e.g., wood paneled) as well as with the principal structure. When not visible from the public right-of-way, modern paneled garage doors may be acceptable.
- ii. *Replacement*—Replace historic outbuildings only if they are beyond repair. In-kind replacement is preferred; however, when it is not possible, ensure that they are reconstructed in the same location using similar scale, proportion, color, and materials as the original historic structure.
- iii. *Reconstruction*—Reconstruct outbuildings based on accurate evidence of the original, such as photographs. If no such evidence exists, the design should be based on the architectural style of the primary building and historic patterns in the district. Add permanent foundations to existing outbuildings where foundations did not historically exist only as a last resort.

*Historic Design Guidelines, Chapter 4, Guidelines for New Construction*

#### 3. Materials and Textures

##### A. NEW MATERIALS

- iii. *Roof materials* – select roof materials that are similar in terms of form, color, and texture to traditionally used in the district.
- v. *Imitation or synthetic materials*—Do not use vinyl siding, plastic, or corrugated metal sheeting. Contemporary materials not traditionally used in the district, such as brick or simulated stone veneer and Hardie Board or other fiberboard siding, may be appropriate for new construction in some locations as long as new materials are visually similar to the traditional material in dimension, finish, and texture. EIFS is not recommended as a substitute for actual stucco.

#### 5. Garages and Outbuildings

##### A. DESIGN AND CHARACTER

- i. *Massing and form*—Design new garages and outbuildings to be visually subordinate to the principal historic structure in terms of their height, massing, and form.
- ii. *Building size* – New outbuildings should be no larger in plan than 40 percent of the principal historic structure

footprint.

- iii. *Character*—Relate new garages and outbuildings to the period of construction of the principal building on the lot through the use of complementary materials and simplified architectural details.
- iv. *Windows and doors*—Design window and door openings to be similar to those found on historic garages or outbuildings in the district or on the principle historic structure in terms of their spacing and proportions.
- v. *Garage doors*—Incorporate garage doors with similar proportions and materials as those traditionally found in the district.

## **B. SETBACKS AND ORIENTATION**

- i. *Orientation*—Match the predominant garage orientation found along the block. Do not introduce front-loaded garages or garages attached to the primary structure on blocks where rear or alley-loaded garages were historically used.
- ii. *Setbacks*—Follow historic setback pattern of similar structures along the streetscape or district for new garages and outbuildings. Historic garages and outbuildings are most typically located at the rear of the lot, behind the principal building. In some instances, historic setbacks are not consistent with UDC requirements and a variance may be required.

## **FINDINGS:**

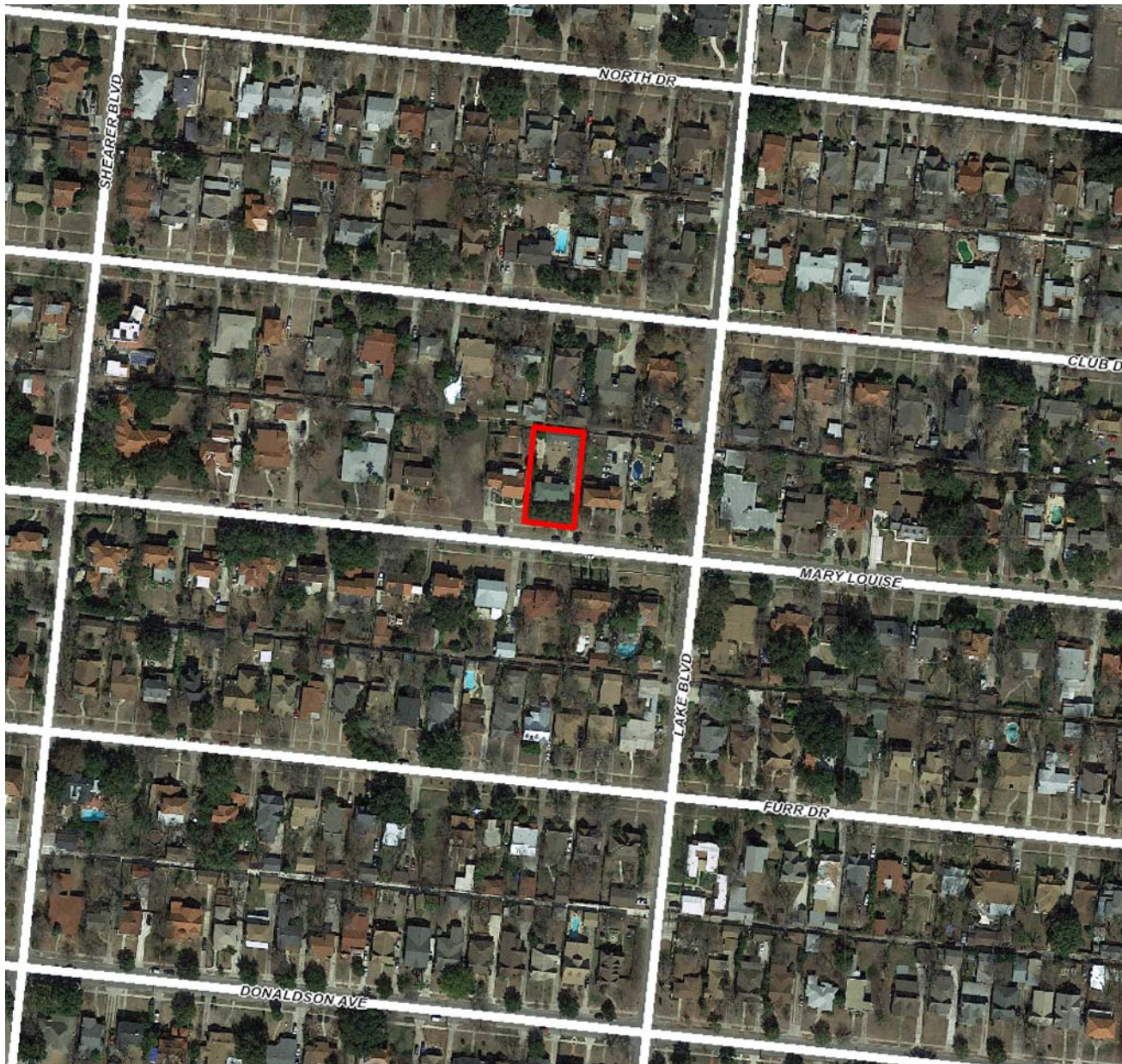
- a. The applicant demolished the existing carport and began to construct a carport prior to receiving a Certificate of Appropriateness.
- b. The applicant constructed a new carport where one previously existed. The previous carport was not original to the property per the 1944 Sanborn. According to the Guidelines for Maintenance and Alterations 9.A., existing historic outbuildings should be preserved and repaired. Because the carport is not historic, staff finds the proposal to remove the pre-existing carport and build a new one consistent with the guidelines.
- c. The primary structure is a two-story stone, Spanish eclectic with minimal detailing and ornamentation with a green asphalt roof and a second story front balcony. There is an existing accessory structure at the rear of the lot, with white shingle siding, a front gable with a 13' ridge, and green asphalt roof shingles; only the garage portion of the accessory structure is found on the 1944 Sanborn.
- d. The previous carport had a flat roof, and was 18' x 17' x 8' and located at the end of the existing driveway, with a 5' side setback.
- e. The applicant is proposing to rebuild the carport in the location of the previous carport with the same access. Staff made a site visit on March 5, 2016, and found that rear garages and carports along the block. This is consistent with the Guidelines for New Construction 5.B, which recommends new outbuildings match predominant garage and carport orientations.
- f. The applicant is proposing a carport with a front gable roof with green asphalt shingles, white hardi plank siding on the front gable, and 10' metal posts. According to the Guidelines for New Construction 3.A., complementary and similar materials should be used; hardie board may be appropriate for new construction in some locations as long as new materials are visually similar to the traditional material in dimension finish, and texture. Staff finds the materials consistent with the guidelines.
- g. The applicant is proposing to build a new carport that is not attached to the garage. The new carport is 22' x 25', 550 square feet, has a front gable roof with a 15' ridge height and a 10' clearance to accommodate a new vehicle. The side setback remains at 5 feet. According to the Guidelines for New Construction 5.A., new outbuildings should be no larger in plan than 40% of the principal historic structure's foot print, and should be visibly subordinate to historic structures. In regards to height and massing, staff finds the proposed carport 15' ridge height overwhelms the historic garage. This is inconsistent with the guidelines. Staff recommends that the applicant consider a flat roof which would be transparent from the public right-of-way and relate to the historic garage.

## **RECOMMENDATION:**

Staff does not recommend approval based on findings g.

## **CASE MANAGER:**

Lauren Sage



**415 Mary Louise**

**Carport**

Printed: Mar 01, 2016

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To Whom it May Concern,

I failed to contact the Historic District regarding the replacement of our carport at my residence in Monticello Park and I regret this oversight. I did not realize the replacement of a free-standing, existing carport would need approval.

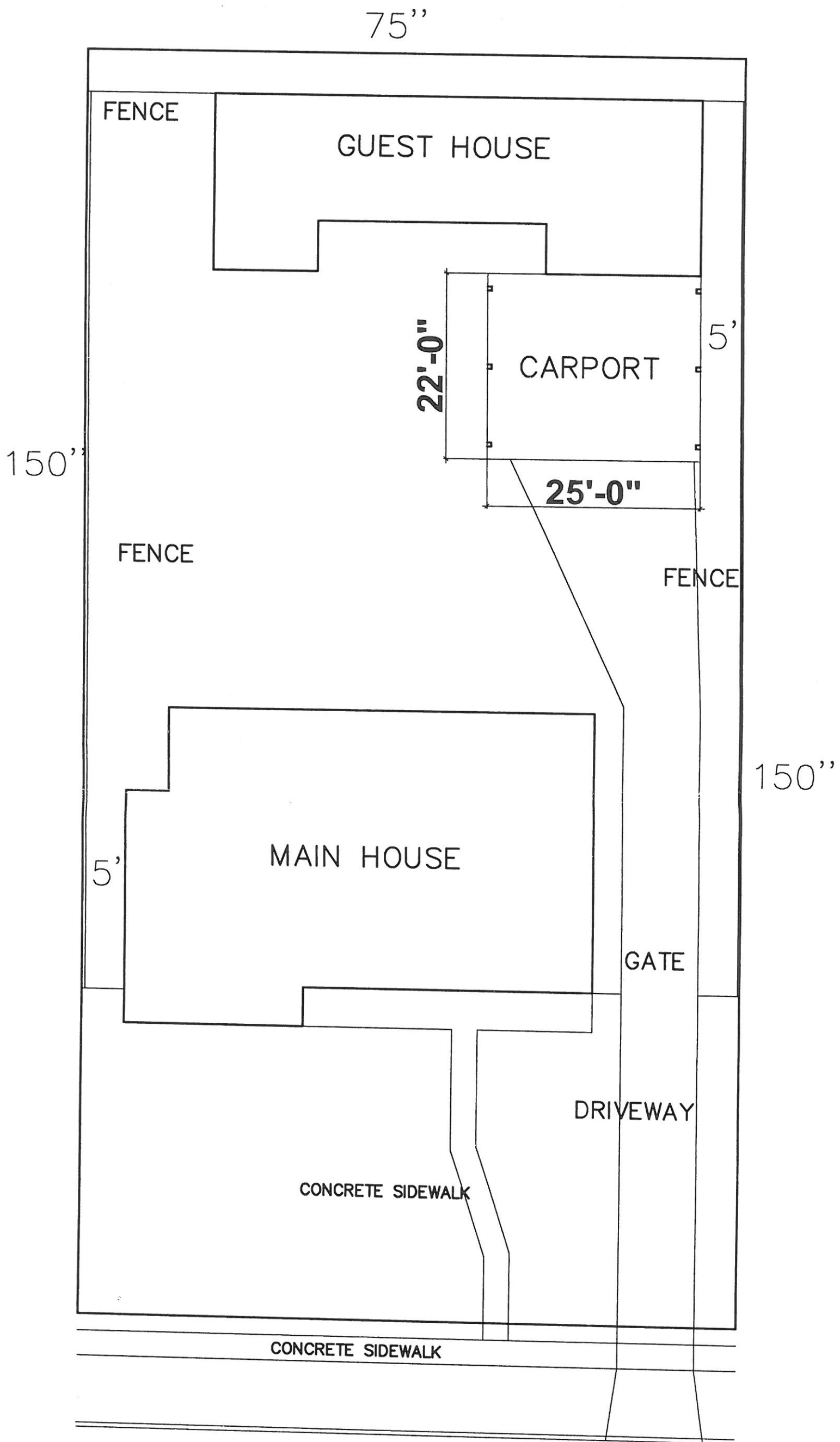
The main need for a new carport arose when we bought a new passenger van that would not fit under the current carport. This required us to go higher than the roof of the garage because the original structure was even with the garage but much too short. Since we were removing the current carport, we decided to extend the carport to a 3 car carport. We own 3 vehicles and have extended family who stay with us often so we were having to street-park. The street-parking poses a danger to our 4 children who frequently play and ride their bikes on the sidewalk in front of our home as well as traffic using the street which sometimes speeds by.

I recognize the height is uneven with the garage but it was necessary to fit the van we bought and the height is actually lower than our backyard neighbor's structure in their backyard. The address of their house is 314 Club Dr. There are many houses on our street and adjoining street with backyard structures as high or higher than the new carport.

We care very much about the historic nature of our lovely neighborhood and do intend to fit this carport into the look and feel. For example, we will be painting the exterior of the carport the same white as the casita and garage behind it and also be using the same kind and color of shingles on both the casita/garage structure as well as the main historic house.

We appreciate what you do to protect what we have in Monticello Park. We love our home and intend to raise our family here.

My Very Best Regards,  
David Whitworth



415 MARY LOUISE  
SITE PLAN





OTW-3848

























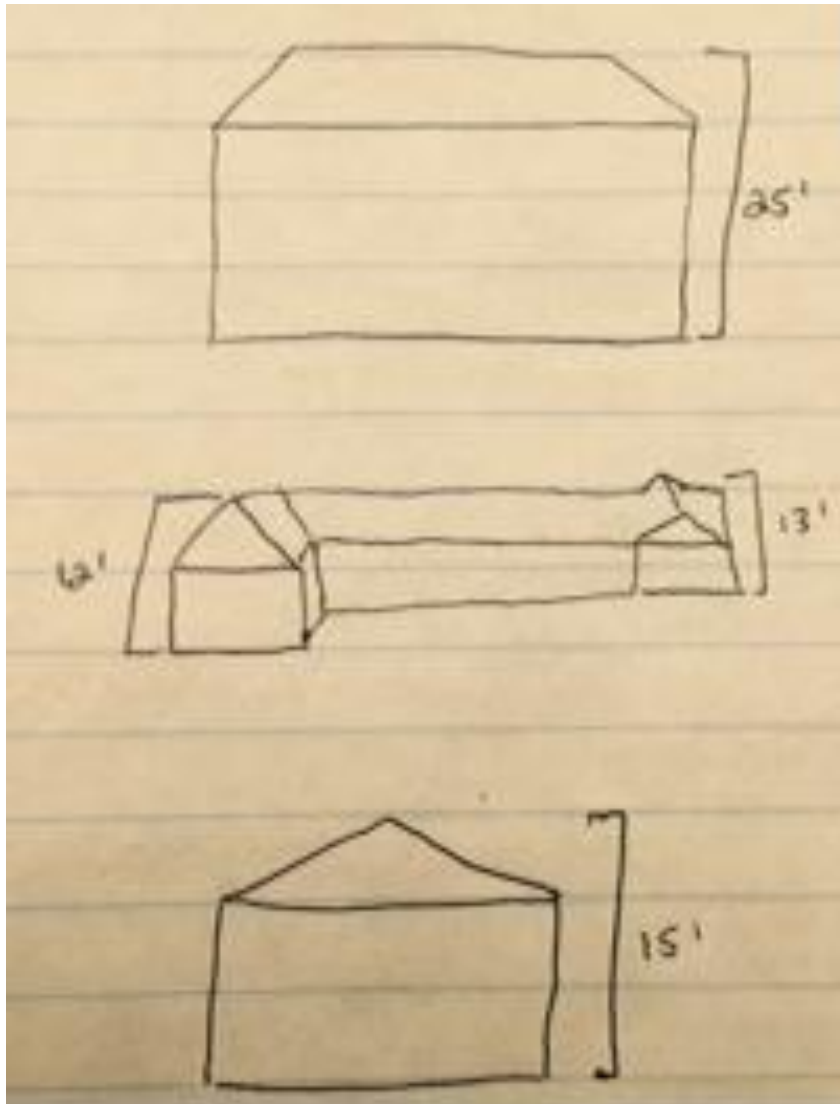








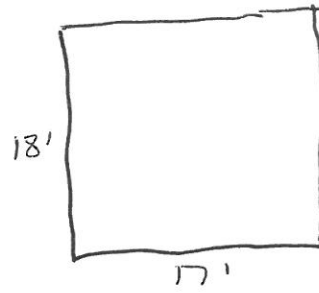
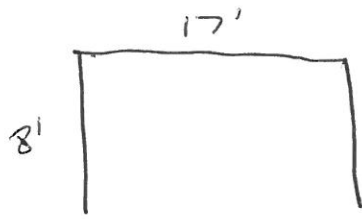




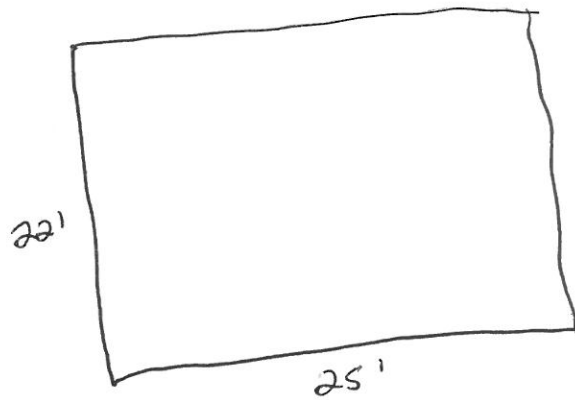
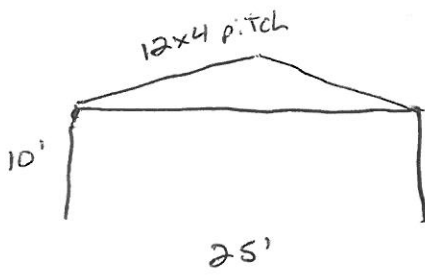
The ridgelines are 12' for the casita, 13' for the garage, 15' for the carport and 25' for the main house.



Old Carport

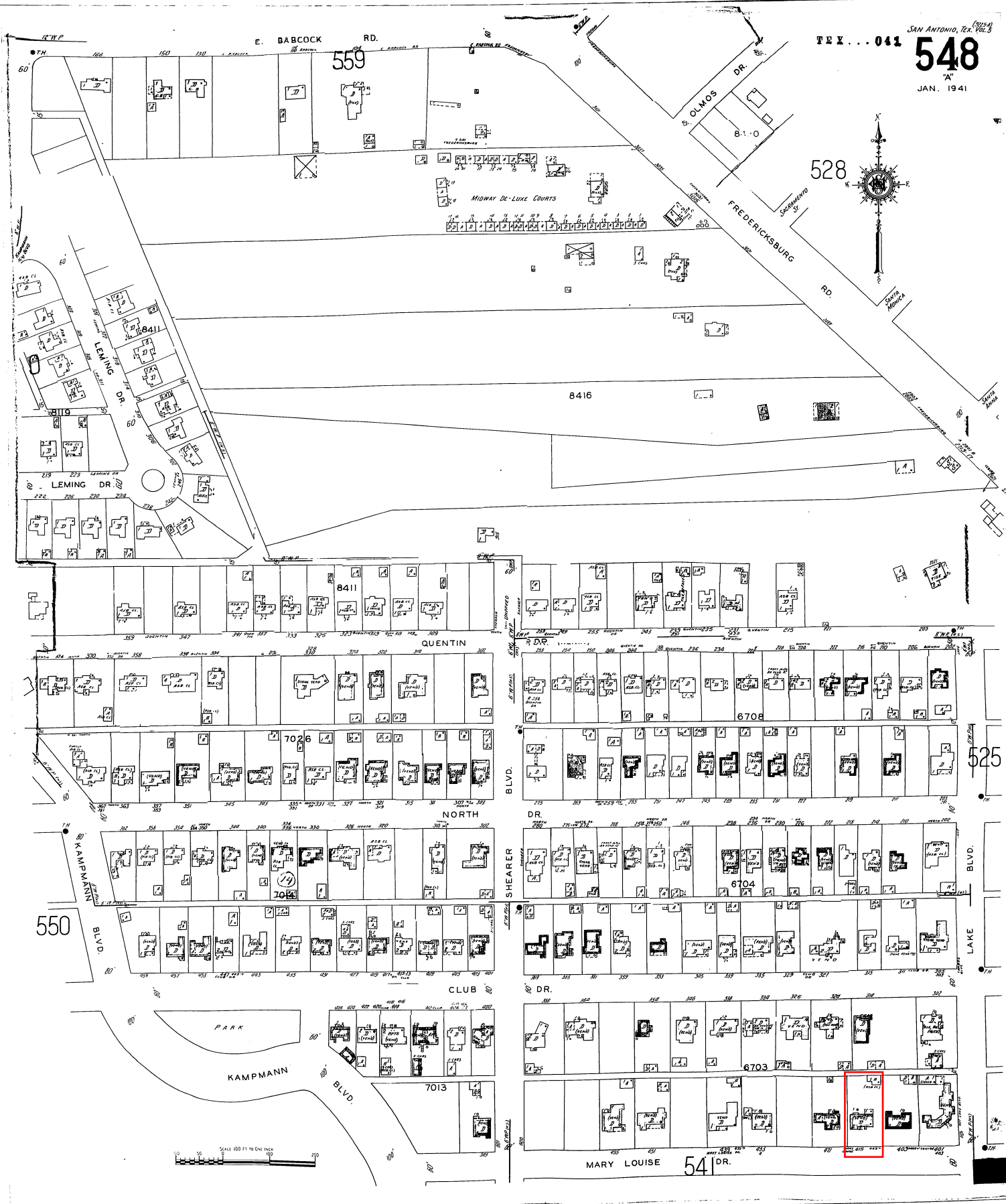


New Carport





TEX...041  
SAN ANTONIO, TEX. VOL. 5  
**548**  
"A"  
JAN. 1941



SCALE 100 FT. TO ONE INCH





March 16, 2016

To Whom It May Concern:

Re: HDRC case no. 2016-095 (carport at 415 Mary Louise)

While the Monticello Park Neighborhood Association is not opposed to the installation of a carport near the rear of a property, we agree with staff's determination that the constructed carport is too large. We support the recommendation for DENIAL. The carport's design should be simplified, and the massing reduced. We also have concerns that rainwater from the currently constructed roof will empty onto a neighboring property. The revised design should ensure that drainage does not impact other properties.

As a matter of precedent, in 2012, the HDRC required a neighbor on the 500 block of Mary Louise to remove a carport that was built without securing required permits and was deemed to be out of scale with the historic property.

Sincerely,

Bianca Maldonado

President

Monticello Park Neighborhood Association

BM/RWS