

City of San Antonio Transportation & Mobility Committee

Broadway Corridor Lower Segment (Houston Street to IH35)
Traffic Analysis Summary
September 16, 2019

Razi Hosseini, P.E., R.P.L.S Interim Director & City Engineer



Broadway Corridor

PROJECT BACKGROUND



Broadway Corridor (E. Houston to E. Hildebrand)



\$97 Million Needed
\$56 Million Funded

Lower Broadway

Houston to IH35
72-ft-78-ft Right-of-Way

\$27 Million

Upper Broadway Phase 1

IH35 to Mulberry
80-ft-100-ft Right-of-Way

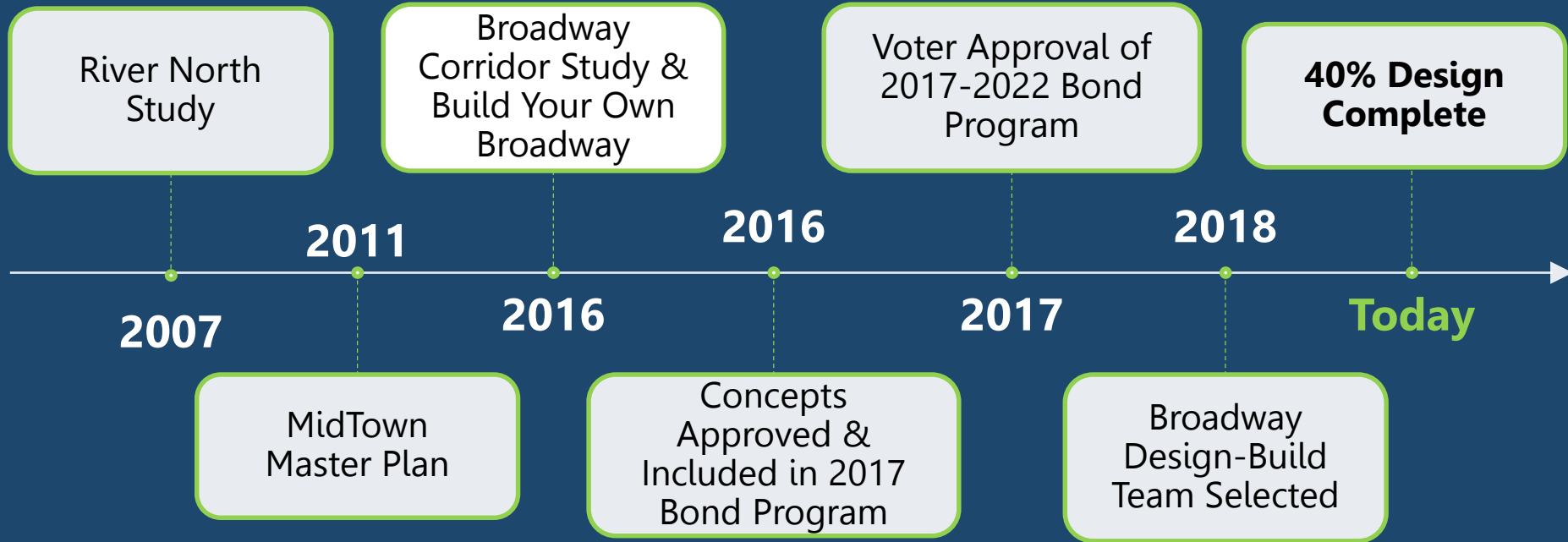
\$28.5 Million

Upper Broadway Phase 2

Mulberry to Hildebrand (Burr)
100-ft Right-of-Way

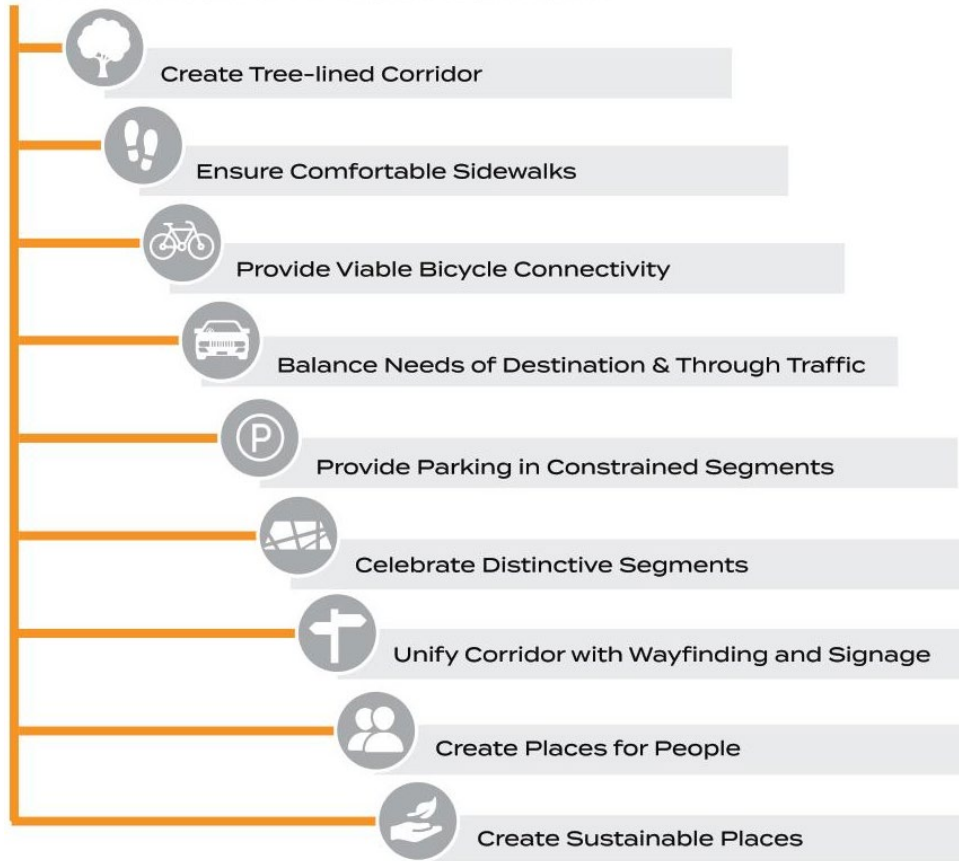
\$0.5 Million

Project History



“Reconstruct Broadway from Houston to Hildebrand with curbs, sidewalks, driveway approaches, bicycle amenities, lighting, drainage and traffic improvements as appropriate and within available funds. City funding will leverage state and federal funding.”

Broadway Guiding Principals



Complete Streets Policy September 2011 Ordinance

- Takes into **account all users**, of **all ages and abilities**
- **"Complete" does not mean all streets must be the same**, the roadway function and level of all traffic modes is considered
- Respects **land use** context, **available right-of-way**, and **cost**
- **Interdisciplinary approach** that combines engineering and planning best practices



Example urban corridor developed by the National Association of City Transportation Officials (NACTO) *Urban Street Design Guide*.

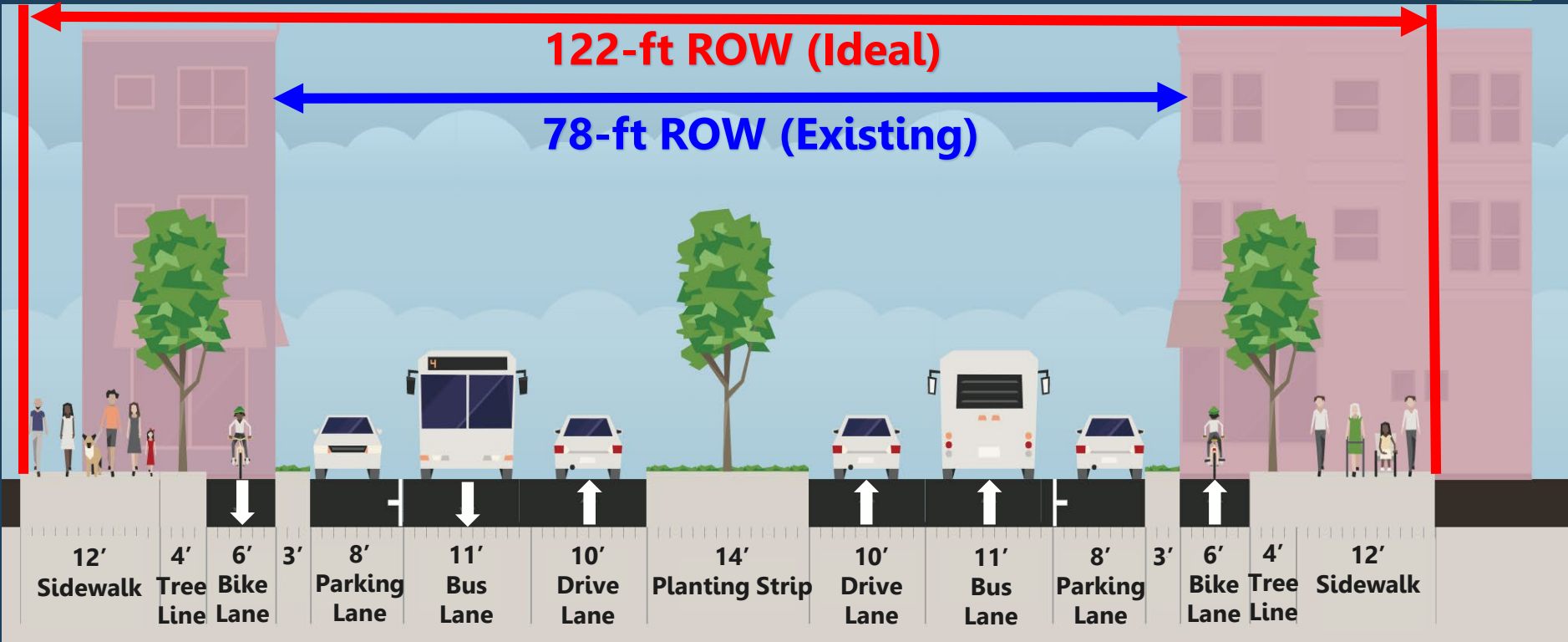
Traffic Analysis

Lower Broadway



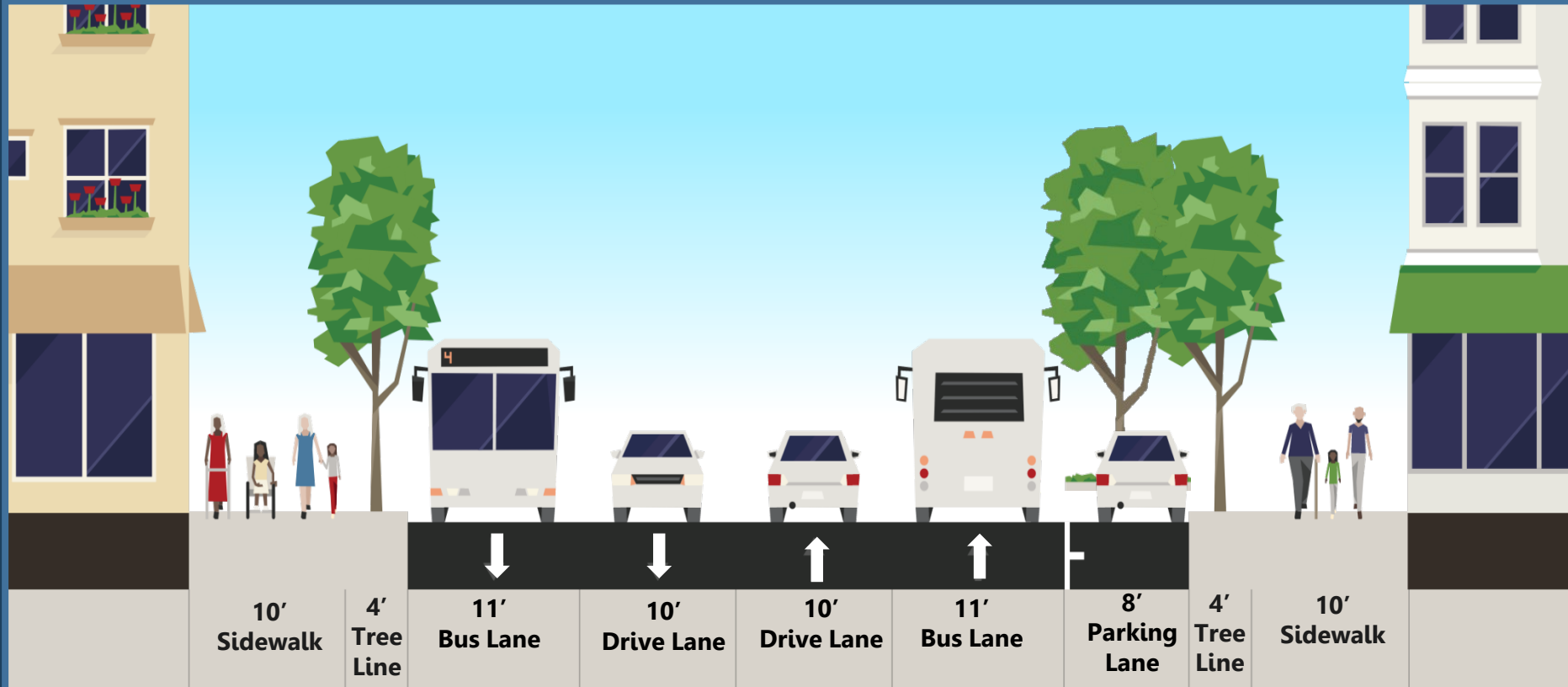
Complete Streets Policy

Lower Broadway Existing Right-of-Way



Note: Upper Broadway does not include the parking/utility lanes shown in the example above and also has a narrower center median to accommodate the 100-ft ROW.

Typical Streetscape Concept Lower Segment (78' ROW Width) South of Josephine

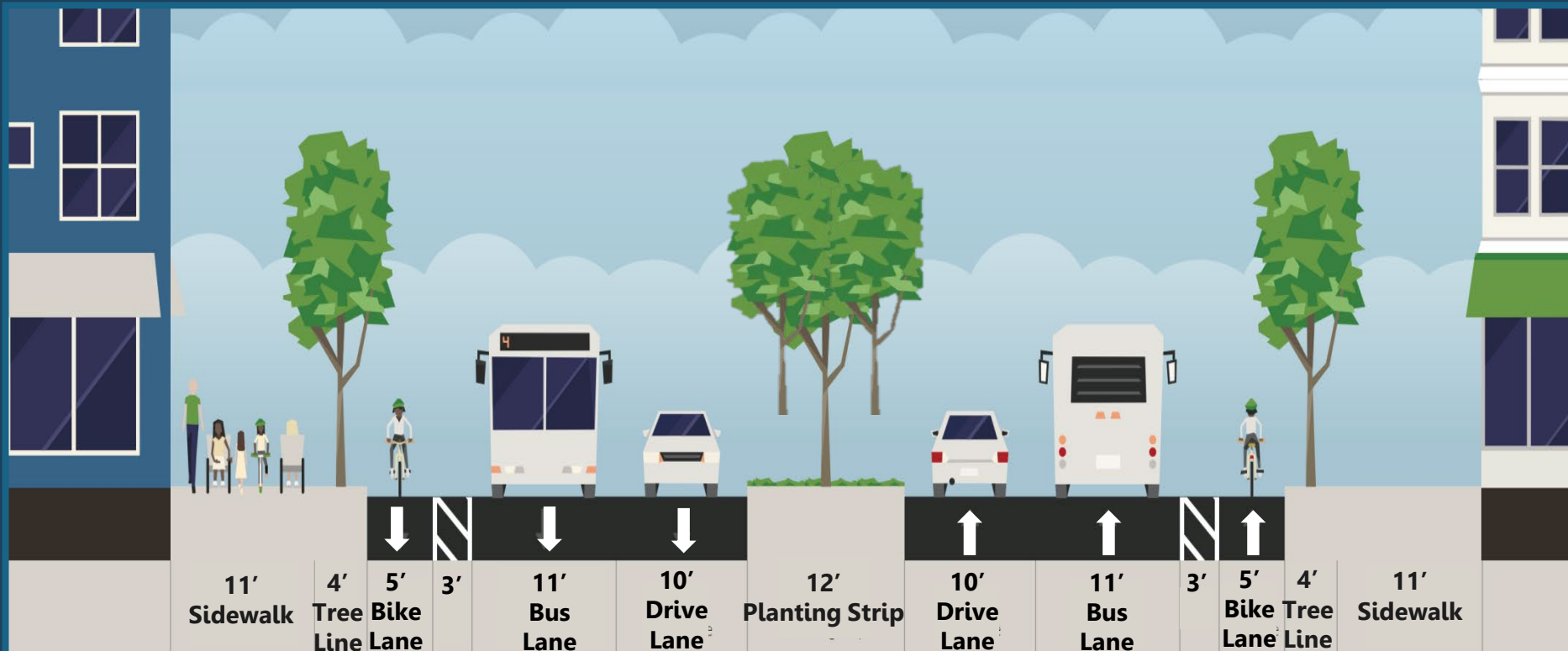


Typical Streetscape Concept



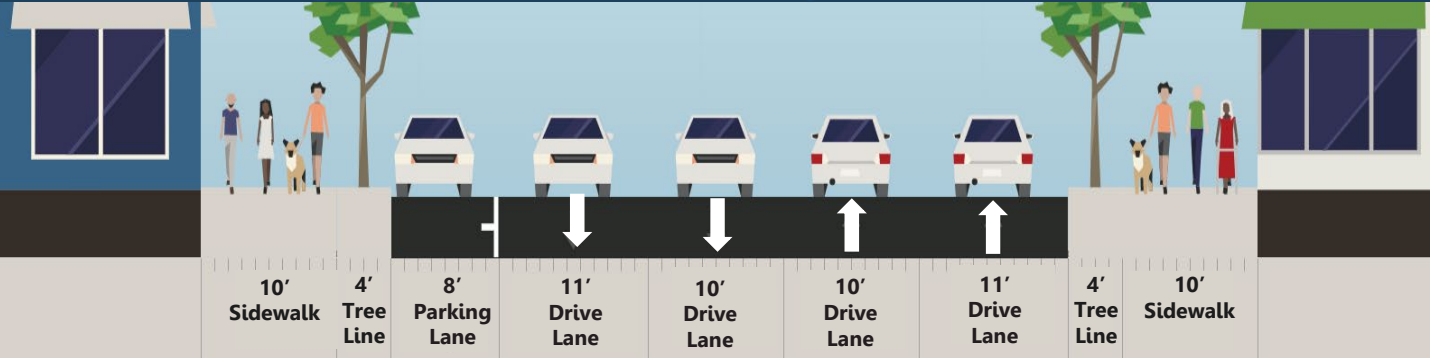
Typical Streetscape Concept Upper Segment

(100' ROW Width) North of Josephine

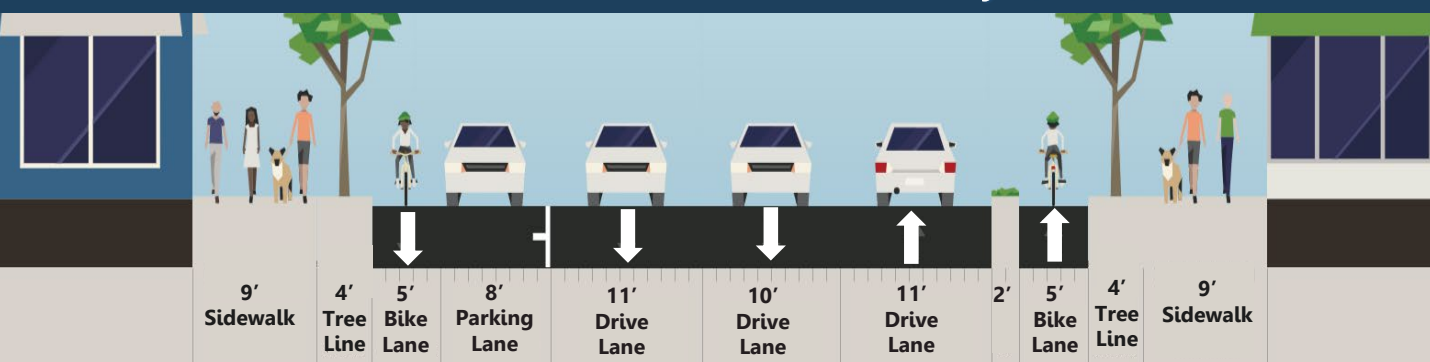


Design Alternatives

Current Design of Lower Broadway



Remove Vehicle Lane & Add Micromobility (Mm) Lanes



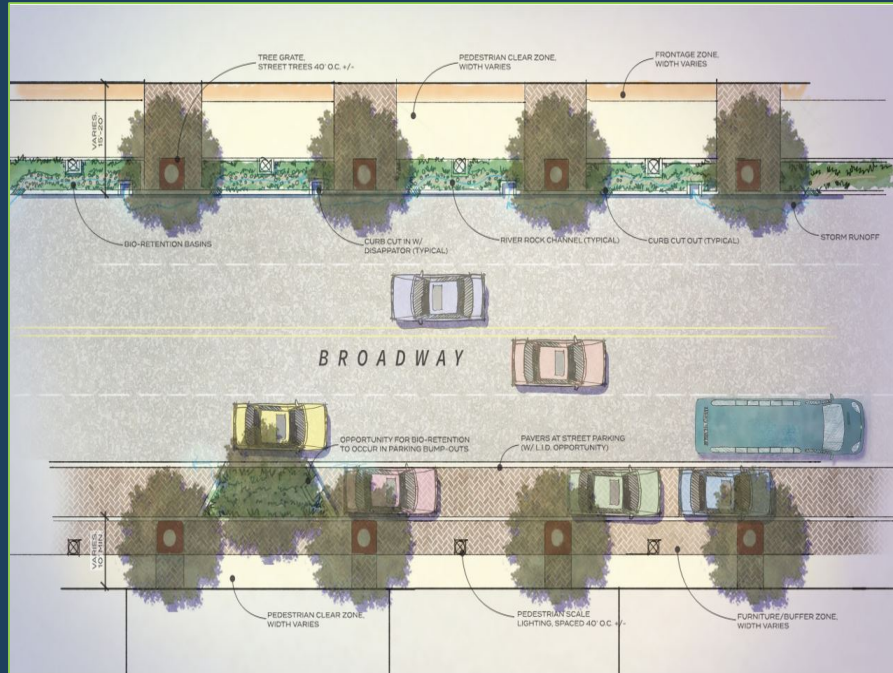
Trade-Offs

Mm Lane Not Protected

Significant Vehicle & Transit Congestion Along Broadway & Intersecting Streets Due to Lane Reduction

Mm Conflict w/Door Zone

Utility Lane Use

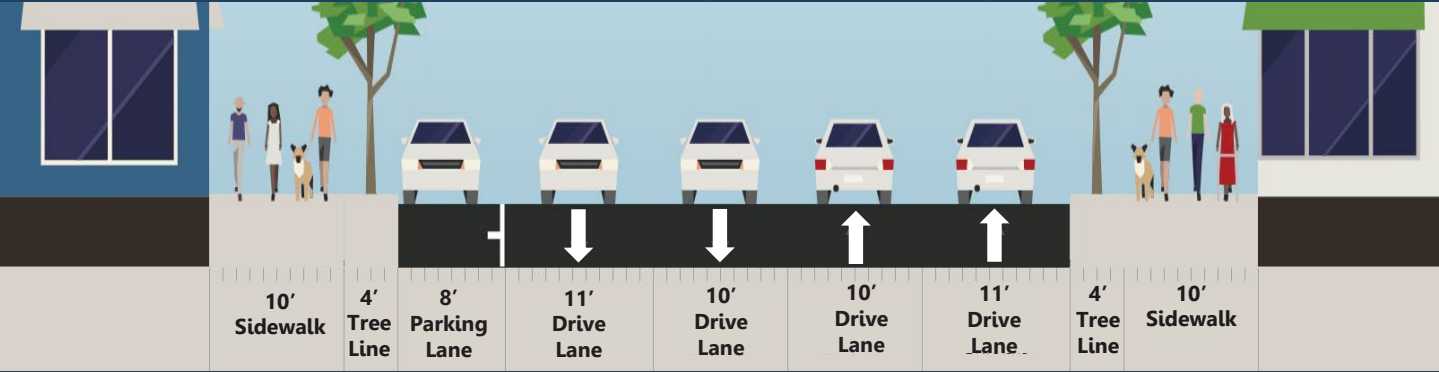


- Delivery & Rideshare
- Retail & Visitor Parking
- Street Trees & Shade

- Low Impact Development (LID) Features
- Enhanced Pedestrian Safety & Comfort

Design Alternatives

Current Design



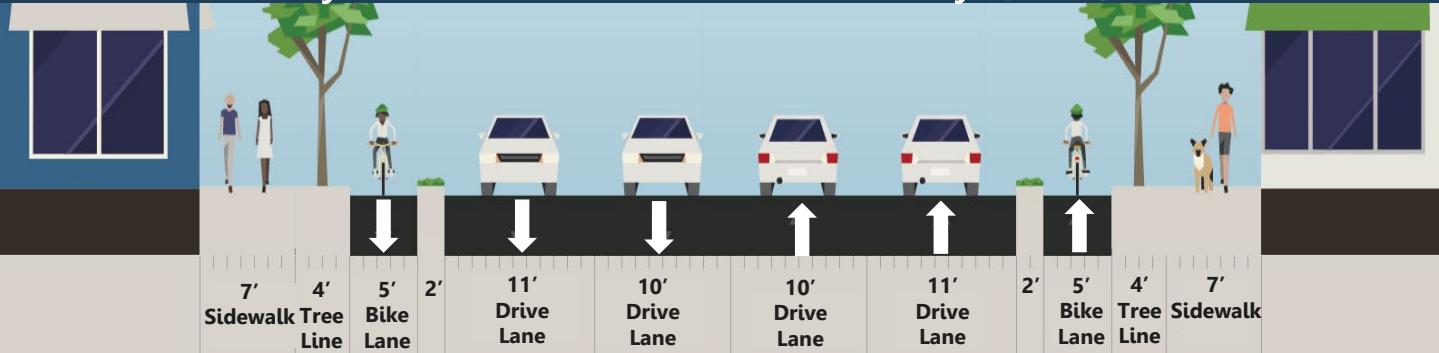
Trade-Offs

Narrower Sidewalks

No Utility Lane & Curb Bulb-Outs





















No Buffer on Mm Lanes near Josephine (Turn Lane Required)

Remove Utility Lane & Add Micromobility (Mm) Lanes





Complete Street Summary

				
Upper Broadway Current Schematic				
Lower Broadway Current 40% Design				
Remove Vehicle Lane & Add Micromobility Lanes				
Remove Utility Lane & Add Micromobility Lanes				

**Does not include Avenue B micromobility facility.*

Level of Service

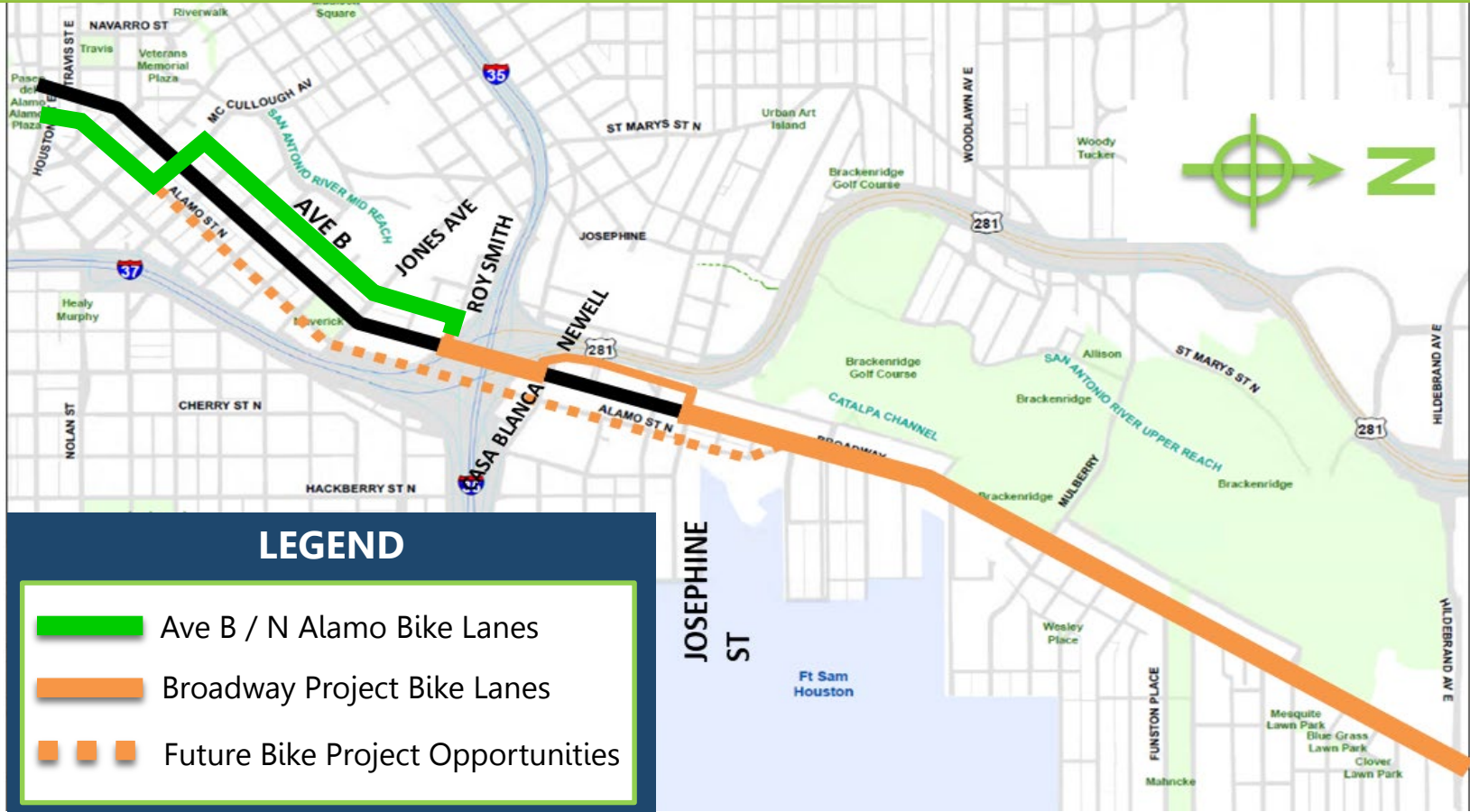


Avenue B & N. Alamo

Bike Lanes



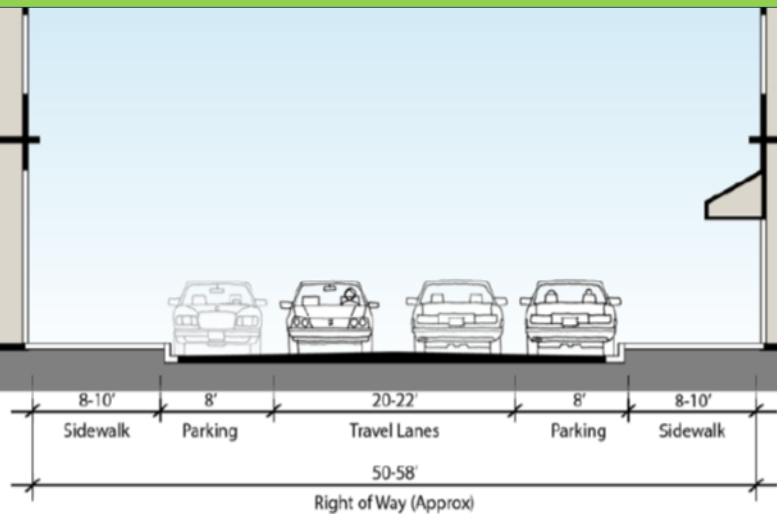
Broadway Bicycle Map



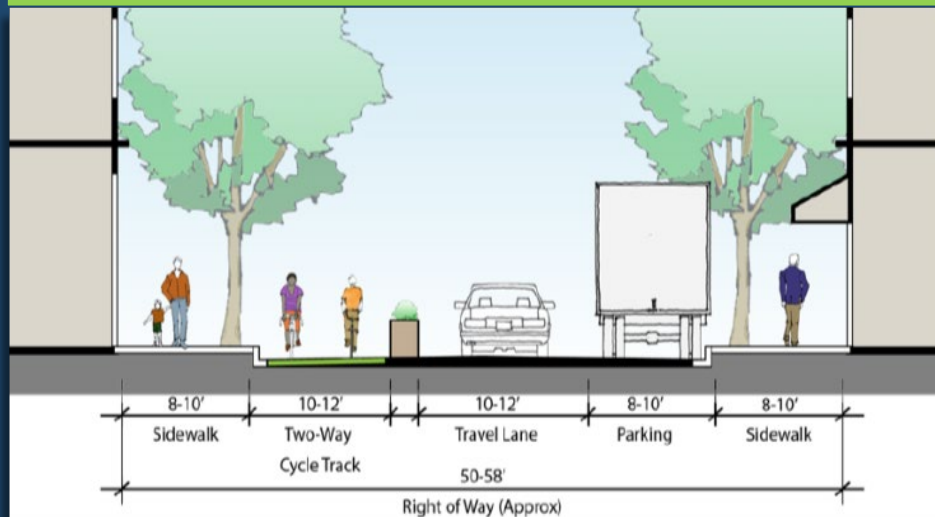
Typical Cross Section

Avenue B – Jones to McCullough (Looking South)

EXISTING



PROPOSED



Scope

- Two-Way Protected Bike Lane
- Landscaping & Trees
- Intersection Bulb-Outs & New Pedestrian Ramps

Avenue B at 9th Street



Schedule

Funding Approved by
Midtown TIRZ Board

September 2019

Design Start

January 2020

Construction Start

January 2021

Construction Finish

Fall 2021





Summary

- Study concluded installation of bike lanes on Broadway causes various mobility challenges:
 - Traffic lane reduction will cause major congestion
 - Narrower sidewalks and reduction of trees
 - Create unsafe conditions for transit users and bikes
- Staff recommends proceeding with the submitted 40% design that includes four lanes on Broadway with on-street parking, drop-off/delivery lanes, wide tree-lined sidewalks and protected bike lanes on Avenue B for safer pedestrian and transit use.

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